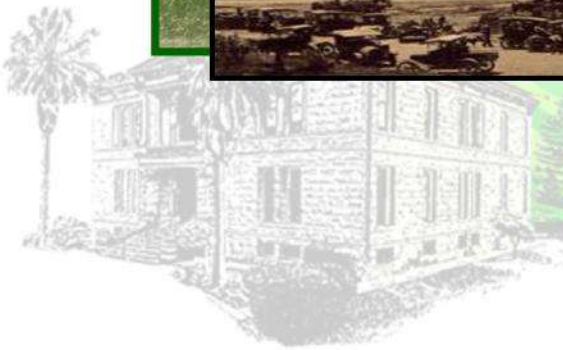


Fairmead Colony Area Plan



FAIRMEAD COLONY

AREA PLAN

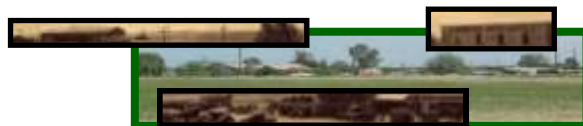
Prepared by:

MADERA COUNTY PLANNING DEPARTMENT

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DRAFT

ALTERNATIVE C REVISED, FEBRUARY 2012



***FAIRMEAD COLONY
AREA PLAN CREDITS***

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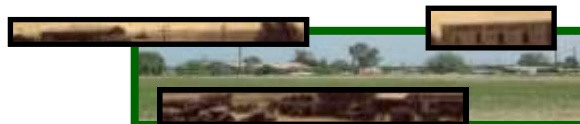


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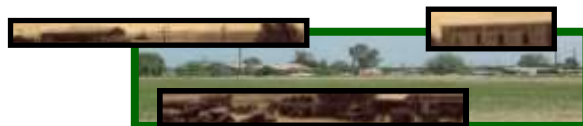
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CHAPTER ~ 1 ~

INTRODUCTION

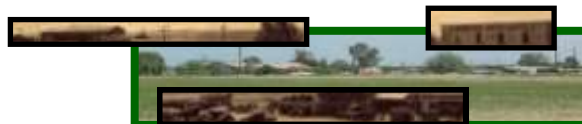
Location

The Fairmead planning area is located along State Highway 99, just south of the intersection of Highway 99 and Highway 152. The entire planning area consists of 3,876 acres (approximately 6 square miles) straddling both sides of Highway 99, focused on the existing community of Fairmead. The City of Chowchilla is located approximately 3 miles to the northwest along Highway 99 and the City of Madera is located approximately 8 miles to the southeast along Highway 99.



Issues Addressed by the Plan

The building blocks of the community, including its infrastructure and community facilities, are either not present in the community or are failing. A combination of small lots and deteriorating septic systems are indicative of the need for a community sewer system. The planning area lacks adequate services including fire and emergency services since each service comes from as far as Chowchilla or Madera. Flooding has also been a problem during heavy rain years, with the southern and western portions of the area within a flood zone. Fairmead currently is a community with a large segment of the population at or below the poverty line, which subsequently increases the need for affordable housing in the Fairmead area, as well as throughout similar communities in the San Joaquin Valley. In addition, the majority of the housing stock is old and in need of rehabilitation. In response to the myriad of challenging issues, the Fairmead Colony Area Plan provides a road map to address these critical problems.



“The First Step towards Serving the Community”

This Fairmead Colony Area Plan (“Plan”) is the end product of a Community Development Block Grant awarded to the County of Madera as an initial step in serving a low income community with a deficient infrastructure system. The grant application specifically calls for the preparation and development of an infrastructure needs analysis, including assessment of the existing water system, sewage system, an assessment of the existing road system, and the development of an overall revitalization plan to address these issues in a comprehensive manner. In response to this direction, the County has responded through the creation of an Area Plan.

Plan Purpose

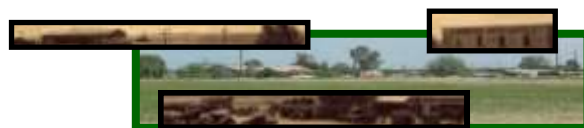
The Plan is essentially a refined land use constitution for the community of Fairmead and surrounding environs, providing policies that address issues specifically pertaining to the Fairmead area. As an amendment to the General Plan, the Area Plan must be fully consistent with the General Plan. The Plan is formed upon careful assumptions, analysis of existing characteristics, and vital public input. Once the Plan is prepared, it must be adopted through a resolution brought before the Planning Commission for review and the Board of Supervisors for final approval.

Related Plans

The Madera County General Plan was first adopted in 1969, and included small area plans for Oakhurst, Coarsegold, Yosemite Forks, and Ahwahnee. The last major update of the General Plan occurred in 1995. The communities of North Fork, O’Neals, Oakhurst, Raymond, Coarsegold, and Ahwahnee/Nipinnawasee all have adopted Area Plans (all except O’Neals and North Fork were adopted prior to 1995). Other areas under development pressure have adopted area plans, including Gunner Ranch West, Gateway Village, Rio Mesa, and State Center Community College. Concerning the Fairmead Area, the County General Plan currently governs land use. Implementation Program 1.1 of the General Plan identifies Fairmead as an area to be targeted for an Area Plan. Area Plans are to be made to address local planning and community facility issues, which are documented in Chapter 2 of this document.

Planned for Growth

Growth will impact the Fairmead Area in the future due to significant developments in close proximity to the Fairmead Area. Approximately three miles to the northwest, the City of Chowchilla has planned for substantial growth through their own General Plan update process. A significant portion of this growth includes industrial uses, which are planned to extend to the northern boundary of the Fairmead Area, opposite Highway 152. The most significant regional impact to the Fairmead area includes the current development of the University of California, Merced approximately thirty miles to the northwest. The University will likely attract various types of development throughout the region, in addition to a highly educated population. The Fairmead Colony Area Plan has been prepared in part to prepare the community and surrounding area for these pressures.



Plan Boundary

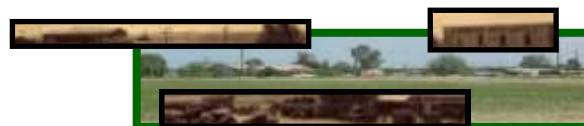
The community includes a small core of compact streets and blocks located east of Highway 99 and the Union Pacific (formerly Southern Pacific) railroad along Fairmead Boulevard. Like many rural places, residents that live well outside this core also associate with Fairmead. In fact, the community’s actual boundary is often disputed.

A community group, Fairmead Community and Friends, proposed a planning area boundary that includes the town core and a large swath of adjacent rural lands. The community group’s proposal encompasses over 9,600 acres, extending beyond Highway 152 and north to Chowchilla’s Sphere of Influence line (at Avenue 25), and between Road 22 and Road 16 to the east and west, respectively. Numerous challenges are presented by this proposal, including conflicts with the City of Chowchilla sphere of influence on its southern boundary, which reaches to the Berenda Slough.



The 1995 General Plan applied boundaries that primarily considered the core of Fairmead, from Highway 99 to Road 20 to the east and between Avenue 23 ½ and Avenue 22 to the north and south, respectively.

The proposed County planning area boundary included in the Fairmead Colony Area Plan provides a medium between the community group’s proposal and the boundaries introduced by the General Plan. More recent changes in the area propose a larger planning area than in the General Plan, which would include the new Avenue 21/Road 20 and Highway 99 interchange that provides Fairmead with direct highway access. The planning area incorporates a larger area that extends further to include areas to the east (to Road 21) and west (to Road 17 ½), but does not extend to the north beyond Highway 152 (utilizing Highway 152 as a northern boundary). The planning area also does not conflict with the City of Chowchilla’s existing sphere of influence and provides a more reasonable area that allows for future growth.



CHAPTER ~ 2 ~

Existing Characteristics

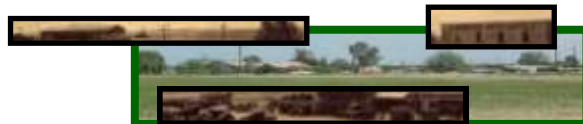
Township History

Fairmead was founded in 1912 by the Cooperative Land Trust Company of Palo Alto as a farming colony. This colony was very similar to others founded at the same time throughout the San Joaquin Valley. Traditionally known as Fairmead Colony, the township blossomed in its early years producing grain, alfalfa, vegetables, fruit, berries, and flowers. By the end of the township's third year, homes, churches, schools, boulevards, parks, and many businesses had sprung up. In this short amount of time in the early 1910s, much of the infrastructure that still exists today was put in place.

The rest of the 1910s resulted in rapid economic growth, feeding off the booming agricultural markets and multiplying local businesses including the Fairmead Inn, a blacksmith, newspaper, lumber company, and many other urban amenities. However, by the 1920s, the town had already begun an economic downspin that reduced most of the town's businesses and residents by the 1930s. The major factor for the town's decline included the rerouting of Highway 99 in 1930 slightly away from the town. The loss of many visitors and the continued growth of nearby towns, especially Chowchilla and Madera, also spurred the township's loss of vitality. By 1940, the township had lost such a large percentage of its residents that the post office was forced to close. Since the 1940s, little change or improvements have been seen in the township, as the community has continued to age and dwindle in size.



*Photo of Fairmead on a busy day
in the 1910s or 1920s
<http://www.cagenweb.com/madera/Fairmead.jpg>*



Existing Land Uses and Site Conditions

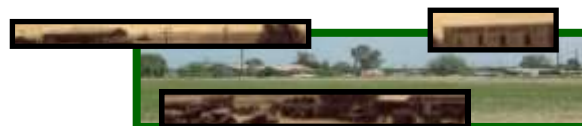
The 1995 General Plan designated the majority of the existing community as Low Density Residential. Three lots are zoned Community Commercial in the middle of the community, along Avenue 22 ½. Fairmead Elementary and Galilee Missionary Baptist Church are both designated as Public Institutional, which allows both uses. Approximately three acres of land abutting the east side of the community are designated for Open Space. The remaining tracts of land surrounding the community are designated as Agricultural, Rural.

The immediate community of Fairmead consists of a collection of small and narrow lots. One hundred of these lots are vacant and the remaining lots have approximately 122 housing units. The housing stock is slightly more aged than the rest of the housing in Madera County, with nearly 39% of the housing stock built before 1960. Large vacant lots surround the developed community, lots which are simply covered with seasonal grasses. Beyond the community lies an established agricultural community, mostly consisting of orchards and other similar agricultural uses.



*The Fairmead Colony Area
Source: GoogleEarth*

The Zoning in the area is largely consistent with the 1995 General Plan designations. The township itself is zoned as “RUS” or Residential Urban Single Family. This zoning allows single family homes to be built on many of the smaller, more urban scale parcels found in the area. A small collection of lots along Ave. 22 ½ are zoned “PCD” or Planned Commercial District and “CUM” or Commercial Urban Median. The large collection of vacant lots and agricultural areas farther outside of the township are zoned “AR-5” (Agricultural Rural – 5 acre minimum), “ARE-20” (Agricultural Rural Exclusive – 20 acre minimum), or “ARE-40” (Agricultural Rural Exclusive – 40 acre minimum). Across Highway 99, some



land adjacent to the highway is zoned “CRH” (Commercial Rural Highway”) and “RRS” (Residential Rural Single Family). The County Landfill is zoned “OS” (Open Space).

Surrounding Land Uses

The entire planning area is surrounded by developed agricultural uses, primarily almond, fig, and stone fruit orchards situated on large 20+ acre parcels. To the north, parcels sizes decrease in size, where some rural residential uses exist. Some industrial uses exist to the northwest on the west side of Highway 99, including a large glass plant immediately north of Highway 152. The City of Chowchilla plans to annex this industrial area, which is designated primarily for heavy industrial uses. The State Women’s Prison is located above the northeast corner of the planning area and is part of the City of Chowchilla. The Prison is a major job center for the entire area and houses thousands of prisoners. The planning area lies at the boundary of the Chowchilla Irrigation District and the Madera Irrigation District. The northern section of the planning area contains Chowchilla Irrigation District canals, whereas the southern section has Madera Irrigation District canals.

Housing

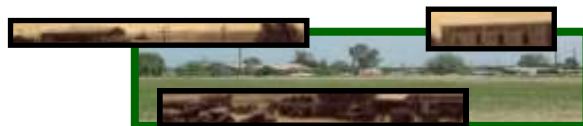
Significant differences exist between housing data for Madera County and the Fairmead Area. Most of these differences expose the need for housing stock rehabilitation and increased housing supply within the Fairmead Area. While the percentage of home owners is less in the Fairmead Area than in the entire County, owner tenure for residents who moved in before 1970 within the Fairmead Area is noticeably greater than in the County. Between 1990 and 2000, average income levels in the Fairmead Area stayed below County levels, with a sharp increase in the very-low income group and an increase in the middle-income group. In the same period, housing has remained unaffordable (owners and renters paying over 30% of income on housing) for nearly a third of all Fairmead Area residents, while County levels dropped significantly. Median home prices also rose substantially in the same period. Concerning the housing stock itself, housing in the Fairmead Area is older than housing in the entire County, 39% of the housing was built before 1960 (compared with 20% in the County). Field visits evidenced a housing stock that appears, in large amounts, substandard. Overcrowding has also developed into a serious issue, as overcrowding in renter occupied units approached 60% from 1990 to 2000. The need and potential for infill housing in the community was also exposed, where many vacant lots exist serviced by existing infrastructure. (2000 US Census)



Figure 4- Home predating 1939



Figure 6- Home not up to building code



Ethnicity

The true diversity of the Fairmead area can be seen through the most recent census data. The area is roughly two-thirds Hispanic, nearly a third Caucasian, and a smaller percentage African American (Figure 10). The diversity of the area is an opportunity—a chance to maintain a diversity of cultures that will add to the community feel and uniqueness of the area.

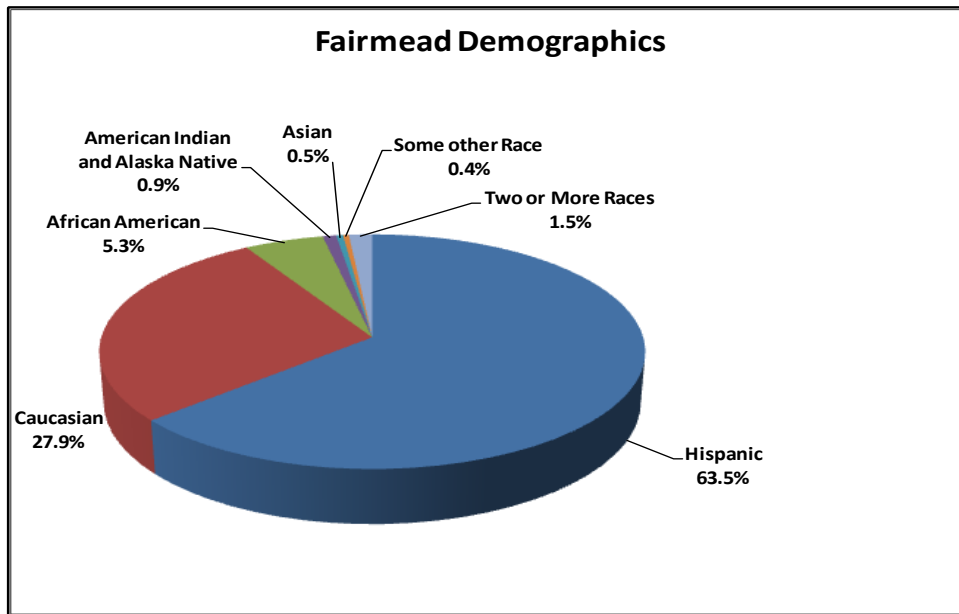
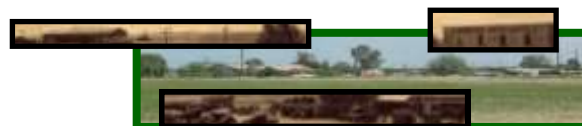


Figure 10- Ethnicity (2010 US Census)

Socio-economic Status

The housing characteristics of the area showcase the low household income levels for the Fairmead area as compared with the rest of Madera County. The low income levels in the area also allude to the low socio-economic status of a significant portion of the population in the area. Thirty-four percent of people in the area are below the poverty level (as of 1999) (2000 US Census). This fact, once again, highlights the need for affordable housing and programs that provide assistance to very low income people. This statistic may also indicate that the imposition of higher district fees to pay for upgraded services such as sewer, water, or flood control may face strong opposition from the community. Therefore, innovative financing plans for the funding of needed community services must be provided.



Elderly

Information on the elderly population is strongly skewed and therefore unreliable due to the potentially high number of elderly women at the women’s prison in the area. However, approximately eight percent of the men in the area were above the age of 65 in the year 2000. It is estimated that at least the same percentage or greater of elderly women are present in the area due to windshield surveys and dialogue with community members. The actual community itself may have an even higher percentage (possibly over 15%) of elderly people than the Census data implies, due to windshield surveys and dialogue with community residents. Significant numbers of elderly people in the area indicates the need for elderly services, such as increased mass transit availability, community activities for seniors, or increased medical services.

Circulation

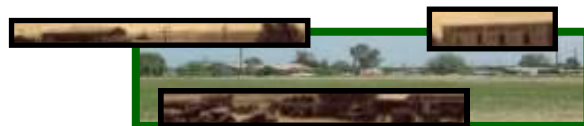
The Fairmead planning area is bisected by Highway 99, which is currently a four-lane, major highway. No major upgrades have been performed on the Fairmead section of the highway since its construction in 1964. Highway 99 provides the main access to communities to the north and south of Fairmead. Highway 152 intersects Highway 99 in the northwestern portion of the planning area, providing access to the western portion of the San Joaquin Valley. The Road 20/Avenue 21 ½ and Highway 99 interchange was recently constructed to allow for safe and direct access to Fairmead. Beginning south of Highway 152, Highway 99 was also recently expanded to six lanes south past Avenue 21 ½. The existing street system of Fairmead consists of a combination of abandoned right of way, poorly maintained paved roads, and unimproved (unpaved) roads. Many of the roads within the planning area were designed and built in the beginning of the 20th Century and have since lacked proper maintenance. Portions of the existing system contain relic curbs, gutters, and sidewalks from the initial development of the community. These relics, however, are now in a state of disrepair due the long-term degradation from lack of maintenance. The Southern Pacific Railroad parallels the west side of Highway 99 and creates a barrier between the highway and the community. The railroad serves freight purposes only. Both the highway and the railroad are the largest noise generators in the planning area, negatively affecting residential properties within 1/8 mile of the Railroad.



Buckling sidewalk



Asphalt deterioration



The road system as planned for the township in the 1910s allowed for large, residential boulevards with landscaped medians and alley access to most lots. The system was designed as a grid network conjoined with a diagonal system in the southern portion of the community. Fortunately, much of the original road network is still intact, at least on recorded maps. The opportunity available is to maintain the existing network and carry it through into the newly developed portion of the community. In this way, the community can literally be designed and built with its historical heritage.



Wide median on Sinclair Drive, unmaintained

The Madera County Connection provides the only fixed-route mass transit service to the Fairmead community and includes a \$2.00 fare. The Chowchilla service, which provides connections between Madera, Fairmead, and Chowchilla, stops 6 times each weekday (from 8 am until 6 pm) at the Missionary Baptist Church in Fairmead. The additional Chowchilla Area Transit Express provides demand-response service between Chowchilla and Fairmead—it runs from 8 am to 3:30 every weekday and includes a \$1.50 fare.

No designated bike routes exist within the Fairmead Area. However, low traffic volumes create an environment that is more conducive to bicycling and many residents have been observed cycling within the community. Yet as traffic increases, there will be a need to identify bike routes for rider safety. It is reasonable to believe that the bicycle will be a favored transportation option in the future for low income groups and children who will need such transportation to get to and from school, shopping areas, and other important community focal points.

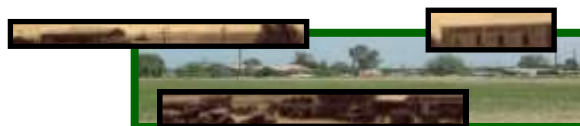
Commute to Work

According to 2000 Census data, sixty percent of people in the Fairmead area travel for fifteen minutes or longer to reach the workplace—this data indicates that there is a strong need for a balance of jobs in relation to housing that is provided. This problem is evidenced throughout the entire County and exacerbates traffic problems on the County's major thoroughfares, such as Highway 99.

Community Facilities

School Facilities

The most vital community facility in the planning area is Fairmead Elementary. The school is located at 19421 Avenue 22 ³/₄, in the northern portion of the existing community. The school is part of the Chowchilla Elementary School District and serves grades three through five. 650 students attend the school, with approximately 90% of the students bused in from outside of the community. The school also provides recreational space for the community. Students from the Fairmead Community in Kindergarten are bused to Stephens Elementary School located at 355 N. 5th St., Chowchilla. Students in grades one through two are bused to Merle L. Fuller School located at 1101 Monterey



Ave, Chowchilla. Students in grades three through four are bused to Ronald Regan Elementary School located at 2200 S. Lake Tahoe Drive, Chowchilla. Students in grades seven and eight are bused to Wilson Middle School located at 1209 Robertson Blvd, Chowchilla. (Note: Grade configurations at specific schools need to be verified with District.)

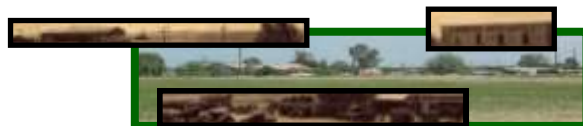
Just as the overall area suffers from inadequate infrastructure and circulation systems, the Fairmead Elementary School also suffers from these same inadequacies. The adjoining streets do not have sidewalks (except for a sidewalk immediately adjacent to a portion of the school site) and the streets are in ill repair and partially not surfaced creating hazards to vehicles traveling to the school. There are unimproved (gravel) rights-of-way adjacent to the school site. Power poles and trees are located in the designated rights-of-way. There is no signalization at the intersection of Maple Street and Avenue 22 ¾, only a stop sign on Maple Street in both the north and south directions. The off-site drainage in the area is generally inadequate causing flooding and ponding of sheet flow water. The water service to the school does not have adequate pressure and flow for the required levels necessary for the operation and irrigation of the school site. In addition the school is on a septic system which should be replaced and connected into an overall sewer system for the area.

These and other similar infrastructure and circulation inadequacies need to be addressed if the Fairmead Elementary School is to adequately serve the residents of the area. Without improvement and expansion of the infrastructure system, the Fairmead Elementary School cannot be expanded and new schools cannot be provided in the area in accordance with the policies, rules, regulations and requirements of the District.

The Chowchilla Elementary School District has been addressing its growth in students by this unique utilization of school facilities with limited grades in each facility and through the recent completion of the Ronald Regan Elementary School. As the Chowchilla Elementary School District continues to increase in enrollment and school facilities are improved, expanded, and developed, the Chowchilla Elementary School District will move towards a more traditional grade configuration arrangement at its schools.

Education for grades nine through twelve is provided by the Chowchilla Union High School District at Chowchilla High School located six miles northwest of the Fairmead Community in the center of the City of Chowchilla. Although Chowchilla High School has been expanded and modernized, the Chowchilla Union High School District continues to require a second high school to serve the future growth of the Chowchilla Union High School District. This school needs to be located in the eastern portion of the Chowchilla Union High School District area that would serve students residing east of Highway 99, including the Fairmead Community.

Fairmead Head Start is located adjacent to Fairmead Elementary on the northeast. Head Start is a large, non-profit national school readiness program. It provides comprehensive education, health, nutrition, and parent involvement services to low-income children and their families. Head Start is another vital community asset that helps support its youth and families. Aside from the school, there is a small “tot lot” (less than a quarter of an acre) at the south corner of Hickory and Arnott Street. The “tot lot” features a small playground structure with a small lawn and benches.



Law Enforcement & Fire Services

The closest Sheriff services are located near the City of Madera. One officer is currently assigned to the Fairmead area, providing 24 hour coverage. The Fairmead area is within the 0-10 minute response time for all fire stations and within 10-14 minutes for the County stations and Amador stations. The closest fire station to the community is located at the Women's Prison. However, the Women's Prison facility does not have guaranteed service as the facility's main obligation is to the prison.

Water & Sewer

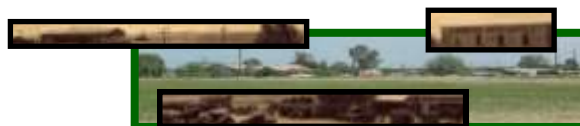
The current Fairmead community water system, Maintenance District 33, serves 166 homes, Fairmead Elementary School, and 71 vacant parcels. Two wells provide supply, producing approximately 200 gallons per minute combined. One well is located on the grounds of Fairmead Elementary, and the other is located near the junction of Maple Street and Yates Street. Madera County has recently completed (2010) an upgrade to the water system, including a 212,000 gallon water storage tank. The storage tank will provide supply during high demand (summer) and provide needed pressure for the community's fire hydrants.



New water storage tank

Fairmead does not have a community sewer system; instead, parcels rely upon individual septic systems. The small lots of the existing community create groundwater contamination issues, as lots with septic systems under one and even two acres typically are not large enough to sufficiently leech effluent. In addition, the development of high water consuming and high waste producing uses (such as more compact or high density development) is curtailed by the lack of a sufficient sewer system with the ability to handle high amounts of effluent.

In 1994, the County acquired a \$1,000,000 Community Development Block Grant to pursue the creation of a sewer system for the community of Fairmead. While the CDBG grant would allow for the creation of the system, Self-Help Housing Enterprises would utilize grant monies for hook-up assistance and individual septic tank removal. Consultant Giersch and Olson conducted a feasibility study on the creation of a sewer system. Three different alternatives were introduced, including aerated lagoons, septic tanks and leach pits, and extended aeration activated sludge. It was determined that the most cost-efficient for the available monies was the system including septic tanks and leach fields. However, as the project development progressed, system costs rose above the initial projected costs. This setback played a significant role in the community's eventual opposition to the project.



In order to enable the Fairmead community to grow, the current water and wastewater systems of the community need to be upgraded and expanded in accordance with the standards of the County of Madera and State of California. The expansion and addition of community facilities, including but not limited to schools, will require these water and sewer upgrades and expansions in order to effectively and efficiently provide the service and facilities sought by the Community. Any further intensity of the utilization of the Fairmead Elementary School will require expanded and integrated water and wastewater treatment systems to replace the current systems used by the facility.

Lighting

At one time, Fairmead had an independently operating lighting district. The former lighting district has since been absorbed by Maintenance District 33 and is now a service provided by the district. While lighting is provided, the amount of lighting within the community is minimal and in need of improvements and service extensions.

Biological Resources

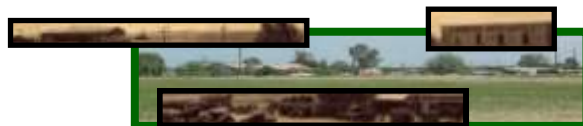
The majority of the Fairmead Area has historically been developed for agricultural purposes, including orchards. The town center has also been developed for a long period of time. Historically, the Area included Valley Grasslands and Vernal Pools. Soil reports from the Madera California State Prison (Women’s Facility) adjacent to the Area indicates the historic presence of both the grassland and vernal pool habitats. Although most of the area has historically been disturbed, the presence of sensitive species associated with these habitats should not be ruled out. These species include:

Plants

- Merced phacelia (*Phacelia ciliate var. opaca*)



Flood impact map of the community



- Greene's tuctoria (*Tuctoria greenei*)
- San Joaquin Valley orcuttia (*Orcuttia inaequalis*)
- Hoover's calycadenia (*Calycadenia hooveri*)

Animals

- California tiger salamander (*Ambystoma tigrinum ssp. Californense*)
- San Joaquin pocket mouse (*Perognathus inornatus var. inornatus*)
- Vernal pool fairy shrimp (*Branchinecta lynchi*)

The sensitive habitats that harbor these species may also be present, including vernal pools, pockets of valley grassland, or seasonal creeks. Individual projects may be required to conduct biological evaluations to identify any sensitive species and associated sensitive habitats that may exist on site.

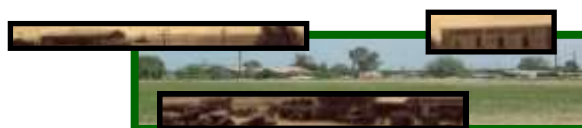
Drainage/Flood Control

The southern portion of the community is within flood zone "A". Zone A includes those areas with a 1% annual chance of flooding and a 26% chance of flooding over the life of a 30-year mortgage. Because detailed analyses are not performed for such areas; no depths or base flood elevations are shown within these zones. The winter of 2004/2005 experienced flooding in this general area, therefore the flooding threat is clear. Studies must be conducted to determine base flood elevations in the area. Field visits confirmed that a number of residences are within the flood zone and are consequently threatened by periodic flooding.

Mitigation measures should be utilized for flood prone areas of the community in order to facilitate flood-safe development. The main channel that contributes to the flood zone has been identified immediately to the northeast of the planning area, running through a pistachio orchard on the east side of Road 20 ½. A strategy for capturing and controlling the channel flow during wet years should be integrated into the Plan. Other mitigations may include the prohibition of development within the flood zone or more reactive measures such as flood proofing, which involves elevating structures or other specific structure protections. However, safeguards must be in place to ensure that mitigations do not cause the displacement of flood water to other parts of the community that are not yet identified as flood prone. Mitigation measures such as simply raising building pads over the flood elevation only serve to displace flood waters elsewhere.

Historical Resources

The Highway 99 Conversion and Interchange Project Environmental Assessment identified three properties that would be eligible to be listed on the National Register of Historic Places. These properties included Laterals 32.2 and 32.2-9.9 of the Madera Canal and the Mammoth Orange Hamburger Stand. The hamburger stand is indicative of similar establishments created throughout the Central Valley during the 1940s and 1950s as attractive travel stops. The Mammoth Orange was removed from the community in 2008 and taken north to the City of Chowchilla.

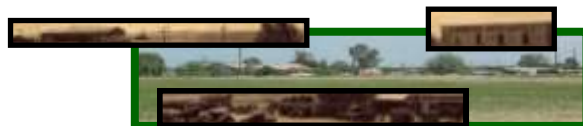


Noise

Both Highway 99 and the Union Pacific Railroad are the major noise generators within the planning area. The parcels most affected are located on the western edge of the existing community, where some residential parcels (also a church) are affected by levels as loud as 65-70 decibels (normal human conversation is measured at 60 decibels). While these noise levels can be considered acceptable by state standards, care should be taken to mitigate any future noise increases in the area (Table 11). Such increases may include increased highway or train traffic. Since 2009 Highway 99 has been moved slightly to the west, which has alleviated some noise issues. Train traffic is expected to remain at existing levels and no reduction in current noise levels are foreseen.

Table 11 - Noise Level Acceptability per Land Use Land Use Category	Community Noise Exposure (Ldn, dBA)			
	Normally Acceptable	Conditionally Acceptable	Normally Unacceptable	Clearly Unacceptable
Residential - Low Density, Single Family, Duplex, Mobile Homes	50 - 60	55 - 70	70 - 75	75 - 85
Residential - Multiple Family	50 - 65	60 - 70	70 - 75	70 - 85
Transient Lodging - Motels, Hotels	50 - 65	60 - 70	70 - 80	80 - 85
Schools, Libraries, Churches, Hospitals, Nursing Homes	50 - 60	60 - 70	70 - 80	80 - 85
Auditoriums, Concert Halls, Amphitheaters	NA	50 - 70	NA	65 - 85
Sports Arenas, Outdoor Spectator Sports	NA	50 - 75	NA	70 - 85
Playgrounds, Neighborhood Parks	50 - 70	NA	67.5 - 75	72.5 - 85
Golf Courses, Riding Stables, Water Recreation, Cemeteries	50 - 70	NA	70 - 80	80 - 85
Office Buildings, Business Commercial and Professional	50 - 70	67.5 - 77.5	75 - 85	NA
Industrial, Manufacturing, Utilities, Agriculture	50 - 75	70 - 80	75 - 85	NA

Source: Office of Planning and Research, General Plan Guidelines, California, October 2003



CHAPTER ~ 3 ~

Plan Vision

Community Involvement

Two workshops were conducted for the purpose of gathering community input to (1) assist in determining the Plan priorities and (2) gather final input on the Conceptual Land Use Map. Community input is one of the most vital ingredients towards developing a Plan that will be the most beneficial to the community it is created for.

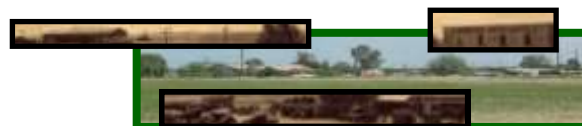
First Community Workshop

Collecting community input is an essential step needed beyond gathering hard data on the existing characteristics of the community. In the initial phases of any Plan, “soft data” is gathered—data that can only be obtained from those who actually live in the community and intimately know the community. For Fairmead, this “soft data” included specific flooding location information, specific crime problem areas in the community, the reliability of the water system, and other highly needed information.

The first community workshop provided the medium through which the studies concerning circulation, community facilities, housing, population, and other issues were presented by Staff to the community. Input was collected through questionnaires and writing on maps of the planning area. The questionnaires were provided to get detailed information on community needs and priorities. During the workshop, community members were encouraged to write all over maps of their community, to identify problem areas, community assets, and ideas for the future. The maps provided an interactive medium through which staff could communicate to community members one-on-one and so gain direct input.

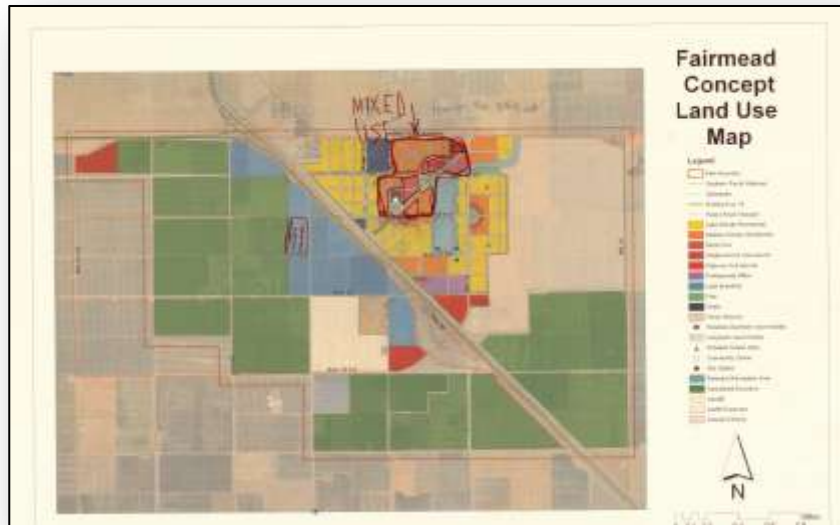


Map with comments from 1st Workshop



Second Community Workshop

Once the design process is ready to begin, further community input is necessary to ensure that the community is designed with the existing community and its needs in mind. The goal of the second workshop was just that, to inform the community of the “whys” and “whats” of the conceptual land use plan and then to gain their feedback. In the beginning of the workshop, the results of the questionnaires and the maps

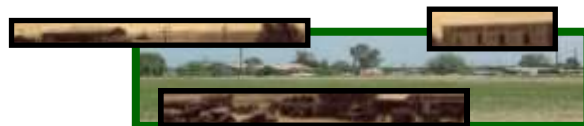


Conceptual Land Use Map for the 2nd Workshop

were presented to the community members. The Plan priorities were also presented—a combination of the “hard” (statistical and other researched data) and “soft” (general community knowledge) data. Finally, in the same fashion as in the first workshop, the community members were given maps (this time of the conceptual land use plan) to review and comment on. Similar to the initial meeting, the break-out session provided direct, one-on-one input from the community.

Community Identity

Fairmead has remained a unique community within Madera County since its inception in 1912. From its beginning as a predominantly African American farming town, Fairmead has grown into one of the most diverse community’s in the San Joaquin Valley. The existing community is largely a mix of African American, Hispanic, and Caucasian residents. The data, along with discussions with residents shows that the community has a higher than average number of people whom have lived in their homes for over 40 years. Furthermore, windshield surveys and data show that homes in Fairmead are, on average, older than those in the rest of the County. All of these factors prove that Fairmead is one of the more historical communities in Madera County that still has a significant historical housing stock in place, along with an older population that has lived in the community long enough to identify with the area’s history.



Planning Area Constraints

Deficient Circulation System

Site visits and discussions with the County Road Department confirmed the poor condition of the road system within Fairmead and throughout the planning area. During the workshops, the community strongly voiced their displeasure with the existing state of the road system and the need to upgrade the system. Not only are the road surfaces in bad condition, but there is a need to improve and continue the existing sidewalks. The community also voiced the need for lighting within the community for safety reasons. Improvements to the circulation system as a whole are a high priority of this Plan.

Distant Employment Centers

The Fairmead Area currently has a limited agricultural employment base. There are no commercial or industrial businesses within the planning area. Agriculture remains the only large employer and business in the area. Community input revealed that most residents who do work commute at least to Chowchilla or Madera for work. A priority of this Plan is to designate for and promote commercial and industrial uses in the Fairmead Area—uses that will provide employment for residents of the area, dramatically reduce commute times, and decrease the impact on the regional road system.

Lack of Community Facilities

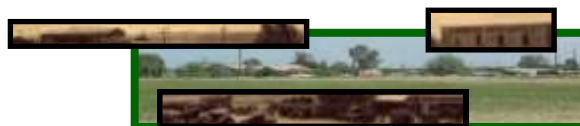
Other important facilities such as parks, a community center, and a fire station are missing. Fairmead Elementary School within the community is aging and there is limited capability to expand the school to accommodate future growth. The lack of such vital facilities in the area is a strong indicator of the poor quality of life that results from such deficiencies.

Deficient Infrastructure System

The community's infrastructure system is either aging or limited in use. Sidewalks are in ill repair and street lighting is limited. Other vital pieces that allow for investment and growth are missing, including sewer and drainage systems.

Stagnant Development Atmosphere

The results from the workshops revealed a desire for growth, revitalization, and investment. The community has experienced only one major development (Self-Help housing, Arnott Street) since its inception in 1912. The community has also been without commercial services since the convenience store at Avenue 22 ½ and the Union Pacific Railroad ceased operation in 2008. In a County that does not have the revenue to pay for all of the services the community needs, private development will have to be the major vehicle that will pay for much of the community's needed improvements.



Flooding

Nearly half of the existing community to the east and south lies in a flood zone. Furthermore, the community was initially planned and constructed without a drainage system. The result is major periodic flooding throughout the flood zone area and localized flooding throughout the rest of the community. The workshops confirmed staff studies of the flood zone and concerns about localized flooding.

Priorities

Community Historical and Cultural Heritage

The community has helped bring to light Fairmead’s rich past as a boom town in the early days of Highway 99. The community has noted the historical importance of the large, palm tree-lined street medians, remaining sidewalks, and story of the town’s booming past. Many residents have conveyed their hopes to one day remake Fairmead into a bustling and vibrant township as it once was. Additionally, many residents have stressed the importance of Fairmead’s unique cultural heritage in relation to the rest of Madera County. Nowhere else in Madera County do you have such a mix between ethnicities—including many African American, Latino, and Caucasian residents.

Growth and Revitalization

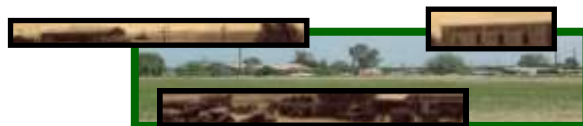
A common saying of many residents reads “*for too long has Fairmead been a forgotten community*”. Little or no improvements have been made to the community for many decades and as a consequence, the built environment and community infrastructure have become substandard. In response to these problems, there is a strong desire to revitalize the community through growth and reinvestment. A major catalyst to economic growth is a sewer system that will allow for intensified growth in the historic core of the community.

Improve Access and Mobility

The new Avenue 21 ½ and Highway 99 interchange has given the community safe, easy access to Highway 99 and the entire region. However, mobility and access within the community are hampered by crumbling streets, discontinuous sidewalks, and a lack of street lighting. The street system must be revitalized to its former glory and improved to present standards.

Jobs/Housing Balance

The community has evolved into a bedroom community of both the City of Chowchilla and the City of Madera. No commercial or industrial businesses remain in the area and therefore, residents rely on employment in other locations. In order to promote economic vitality within the area and reduce car trips outside of the community, it is important to ensure the growth of a strong industrial and commercial base in the area.



Basic Community Facilities & Services

New and improved community facilities are needed, including parks, community center, and increased emergency services. Improved school facilities will be needed. Increased grant activity and growth could be the catalysts to fund and maintain new community facilities and services.

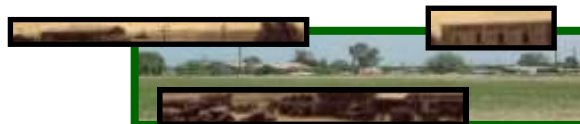
Drainage/Flood Control

After an intense rainstorm, water ponds at intersections and other street low spots. The southeastern edges of the community are particularly vulnerable to flood waters in a heavy rain year. A comprehensive drainage system is needed in order to facilitate the proper drainage of the existing community and to allow for additional growth.

Plan Vision

The vision for the Fairmead Colony Area Plan is the end goal, or vision, of the community after the Plan has been fully implemented. The vision was developed from community input received during the workshops and background data gathered by County staff.

In the future, Fairmead is a community that recognizes and supports its diverse population, history, and cultural heritage by integrating the historic community into a larger, thriving township of the 21st Century. Central to development of the township is an efficient infrastructure system and community facilities that support traditional neighborhoods and a bustling town center modeled after the community oriented villages built in the first half of the 20th Century.



CHAPTER ~ 4 ~

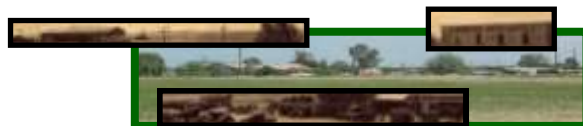
Land Use & Design

1995 General Plan

The 1995 General Plan envisioned Fairmead with the same land uses present in the community since its inception in 1912. The 1995 vision for Fairmead included only two small parcels (two acres) that make up the commercial center of the community, a scattering of single family residential, and an area in the center of the community designated for park space. In 2010, the designated commercial areas now lack businesses, many homes have become abandoned, and the park space has yet to be developed.

1995 General Plan - Land Use

Land Use	Consistent Zone Districts	Area acres	Avg.Density dwellings/acre	Number of Dwellings at Build Out
Residential				
Low Density (LDR)	RUS	194	5 DU	970
Rural Residential (RR)	RRS-2	79	1 DU	41
Rural Estate Residential (RER)	AR-5, RRS-5	116	0.4 DU	46.4
Commercial				
Community Commerical (CC)	CUM, PSC	2		
Open Space				
Park (OS)	POS, OS	4		
Public Institutional				
Community Facilities (PI)	IA	15		
Agricultural				
Agricultural Rural (AR)	AR-5, ARE-20	429	0.2 DU	85.8
Total Acreage		839	Total Dwellings	1,143.20
Total Population		3,658		



Land Use Concept

The Fairmead Area will be composed of a diverse range of land uses, including varying residential densities and commercial uses, an industrial component, public uses, open space, and agricultural areas. Fairmead is to be primarily a community consisting of residential land uses, varying from low density residential to high densities within a mixed use core. Just like a traditional pre-World War II community, a center of mixed commercial and residential uses composes the core. This area provides space for community gathering and other community activity. Employment areas are provided in the form of professional office and light industrial areas to help balance housing provided with jobs—a jobs/housing balance helps to minimize the need for residents to commute to cities such as Madera or Fresno for employment. Agricultural areas surround the planning area to the west and south on the community's lower density fringes.

Planning area Land Use Designations

Low Density Residential (LDR) allows for single family detached and attached homes, bed-and-breakfast establishments, public and quasi-public uses, and similar uses. Densities range from 1 to 7.5 units per acre.

Medium Density Residential (MDR) allows for single family detached and attached homes, duplexes, triplexes, quadplexes, garden apartments, mobile home parks, cottage-style housing, group quarters, bed-and-breakfast establishments, public and quasi-public uses, and similar uses. Densities range from 5 to 12 units per acre.

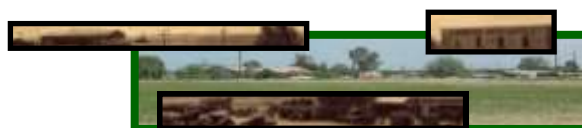
Mixed Use Core (MUC) allows for residential, commercial, and office uses integrated together. In Fairmead, this designation is applied to the town center, the future community focal point of the area. Multiple family units, retail, restaurants, services, administrative and professional offices, and similar uses are allowed. The Fairmead Town Center is 38 acres in size, comprised of a predominately mixed use core, complimented with park space and government/community uses. Residential densities range from 12 to 30 units per acre. Public and quasi-public uses are also permitted.

Highway Service Commercial (HSC) allows restaurants, service stations, truck stops, hotels and motels, and retail and amusement uses that are oriented to serve highway traffic. Public and quasi-public uses are also permitted.

Professional Office (PO) allows for professional and administrative offices, medical offices and clinics, hotels and motels, public and quasi-public uses, and similar and compatible uses. Multiple family residential uses are allowed with discretionary approval, with densities from 12 to 25 units per acre.

Light Industrial (LI) allows for industrial parks, research and development, warehouses, light manufacturing, general commercial uses, professional offices, airports and airstrips, outdoor theatres, public and quasi-public uses, and similar uses.

Agricultural Exclusive (AE) allows for agricultural uses, timber production, mineral extraction, recreational uses, and support services and structures. The minimum parcel size is 40 acres, gross (36 acres, net), allowing one or two primary single family homes and additional caretaker or



farmworker housing. Williamson Act contracts should be encouraged on AE properties used for agricultural purposes. Public and quasi-public uses are also permitted.

Agricultural Rural (AR) allows for more limited agricultural uses as compared to AE. Allows two single family homes. Parcel sizes may be as small as 10 acres. Public and quasi-public uses are also permitted.

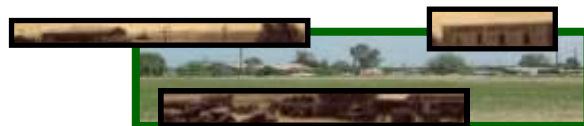
Open Space (OS) allows for low intensity agricultural uses, grazing, forestry, recreational uses, golf courses, habitat protection, watershed management, public and quasi-public uses, mining and other areas where residential uses should be minimized. Community park space, the County landfill, and retention basins are included in the designation within the planning area.

Public Institutional (PI) allows for institutional uses such as colleges, hospitals, sanitariums, libraries, government offices and facilities, churches, community centers, cemeteries, and mausoleums, parks and recreation centers and similar uses. All public and quasi-public uses are also permitted.

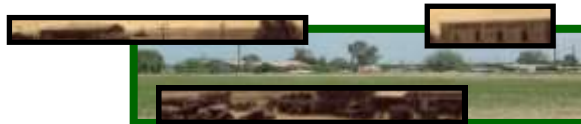
Urban Reserve (UR)

Approximately 557 acres on the eastern edge of the planning area are set aside as an urban reserve. The urban reserve area can be viewed as a growth management technique—allowing a large portion of land to be set aside and “banked” for possible future development. The designated land is also too far away from the existing community to warrant any current development—such development would not aide in creating a cohesively bound community. Lastly, the majority of the urban reserve area contains agricultural land that is currently enrolled in the Williamson Act agricultural preserve program, which limits development for at least the next ten years.

The urban reserve shall be deemed adequate for development once the currently planned community has reached 75% build-out (or 2,340 residential units within the planning area) and the proper community services and facilities are adequate to accommodate additional growth. Once these parameters can be satisfied, any development within the urban reserve area requires an amendment to the Area Plan that is consistent with both the General Plan and Area Plan.



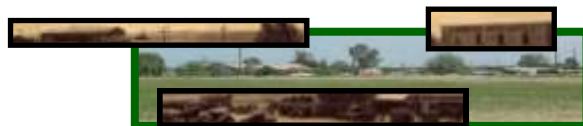
Land Use Diagram



Fairmead Colony Area Plan – Land Use

Land Use	Avg. Density dwellings/acre	Acres	Dwellings at Build Out
Residential			
Medium Density Residential (MDR)	10 DU	93	930
Low Density Residential (LDR)	5 DU	222	1,110
Commercial			
Neighborhood Commercial (NC)	0	21	0
Highway Serving Commercial (HSC)	0	184	0
Professional Office (PO)	15 DU*	25	375
Mixed Use			
Mixed Use Core (MUC)	15 DU	27	405
Industrial			
Light Industrial (LI)	0	359	0
Open Space			
Retention/Recreation Open Space (RROS)	0	52	0
Open Space (OS)	0	3	0
Land Fill Open Space (LFOS)	0	149	0
Public Institutional			
Public Institutional (PI)	0	34	0
Agricultural			
Agricultural Rural (AR)	0.2 DU	466	93
Agricultural Exclusive (AE)	0.06 DU	1,989	119
Urban Reserve	0	561	0
Right Of Way, MID, & CID Canals	0	496	0
Total Acres		4,678	3,032 DU
Population	2.993 persons/DU**		9,075
Schools			
High School	1/10,150 units		1
Middle School	1/10,000 units		1
K-5 Elementary School	1/1,500 units		2***

*Units allowed with discretionary permit, ** Department of Finance 2009 estimate, ***Includes existing Fairmead Elementary



Development Concept

The concept for the design of the planning area is derived from a number of elements, including:

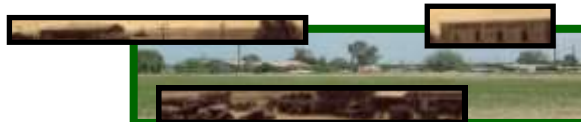
- The design of the existing community
- Existing edges, such as canals and highways
- Drainage and flooding patterns in the area

These design elements are “designed into the Plan”, by creating a land use plan that turns what can be major constraints into opportunities. These opportunities then contribute to the uniqueness of Fairmead and combine to create a sense of place that is unique to the community. For example, the existing street pattern of the community is wholly unique to Fairmead and must be carried through into new areas to maintain the uniqueness of the area.

Other design elements important to the creation of a “sense of place” in Fairmead are community attributes that can be easily identified. The existing community lacks many of these attributes, including park space and pedestrian pathways. The attributes that do exist include most prominently the elementary school, Galilee Missionary Baptist Church, and palm tree-lined street medians.



*Conceptual village core plaza
Source: Opticos Design, Inc.*



Village Core

The creation of a village core is important to the sustainability of Fairmead as a community. Community revitalization also depends on a lively commercial and residential town center that provides commercial services, community activity, and living area. A compact village is essential to the creation of a main destination that draws the community and visitors together to create a unique “sense of place”. The majority of the existing community shall serve as the village core.

Commercial Development

The need for commercial services within the planning area is very high, due to the current lack within the existing community. However, a lack of commercial services does not indicate a need to spread commercial uses throughout the entire planning area. Doing so would create very limited pedestrian and community gathering opportunities. In order to promote modes of transportation other than the automobile, commercial centers must be clustered around special hubs throughout the planning area. These special hubs include neighborhood centers with higher maximum densities than the typical single-family tract home development. Highway oriented commercial areas are located at highly trafficked points on the edges of the community. Such commercial uses will cater specifically to highway customers, including services such as fast food restaurants, gas stations, and motels. Village core and neighborhood commercial areas will specifically cater to community residents. Village core uses include a high density of sidewalk cafes, offices, customer services establishments, and retail sales establishments—all commercial uses that can promote pedestrian activity. Neighborhood commercial uses include a limited amount of grocery stores, offices, customer services establishments, and retail sales establishments.

Industrial Development

A proposed industrial district spans along the west side of Highway 99, intended to provide light industrial manufacturing, agricultural services, office parks, highway oriented commercial, and currently holds the County landfill site. This area is designated as an employment area for many of the residents within the community. The Union Pacific Railroad and Highway 99 adequately buffer potentially noisy and odorous uses within the district from residential areas on the west side of the highway.

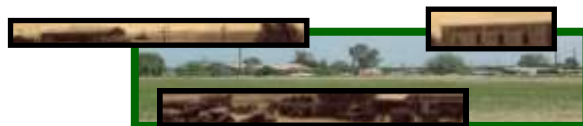
Land Use - Goals, Objective, and Policies

Goal: Create employment opportunities within the planning area through the provision of commercial and industrial land uses.

Objective LU-1: Provide strategically located basic commercial services to the community.

Policy 1: Provide a mixed-use, community “mainstreet” that will provide basic commercial opportunities and a community hub. Such basic services include laundromats, offices, restaurants, retail, small grocery, and customer service establishments.

- Provide mixed use core designations at the core of the community along with the Village Core Overlay.



Policy 2: Create neighborhood commercial nodes that allow for larger commercial services, including grocery stores, home supply, and other similar uses.

Policy 3: Preserve existing commercial lots in the community due to their importance as historical points of commercial activity.

- Two commercial lots exist along Avenue 22 1/2, including one that has been granted a CUP for a small restaurant and a small grocery store.

Objective LU-2: Take advantage of the community's location along major state routes in creating highway commercial opportunities.

Policy 4: Create an area of highway serving commercial land uses on both the northeast and southwest sides of the Avenue 21 1/2 and Highway 99 interchange. Uses encouraged include gas stations, restaurants, hotels, and other uses attractive to motorists.

Policy 5: Create landmarks and gateway signage to attract highway users to the Fairmead area, including to future community highway commercial opportunities, and the Paleontology Museum.

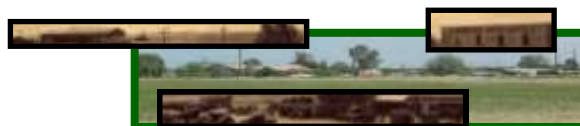


*A tower landmark may be utilized to identify Fairmead from Highway 99.
Source: Opticos Design, Inc.*

Policy 6: Billboards shall be prohibited within the planning area.

Policy 7: Commercial and industrial complexes should limit the amount of freestanding signs utilized and are encouraged to consolidate signage onto a single sign.

Policy 8: Sound walls must be incorporated as development and other improvements occur within the highway corridors. Sound walls must offer aesthetic features that reflect the local identity of Fairmead or Madera County, or landscaping may be incorporating to discourage



graffiti. In particular, Fairmead Boulevard will feature a sound wall along the western boundary of the street, from the northern boundary to the Avenue 21 ½ interchange in the south.

Objective LU-3: Provide an industrial base in the planning area that supports local jobs and the regional agricultural base.

Policy 9: Locate light industrial opportunities on the western side of Highway 99, adjacent to the County’s agricultural base. Uses promoted include agricultural processing, packaging, wood chipping/mulching, uses related to the Fairmead Landfill, and business parks.

Goal: Allow for new development that will support and spur investment within the existing community.

Objective LU-4: Allow new development in and around the existing community to increase community gathering and overall investment within the community.

Policy 10: Promote the logical expansion of residential development to the east of the community (away from Highway 99 and planned circulation routes).

Policy 11: Require all off-street parking to locate to the rear of the buildings instead of between the right of way and buildings. Access to stores and streets should be easily visible from all areas of the parking lot.

Policy 12: Shared parking is encouraged for the efficient use of parking areas and the overall reduction in parking stalls and entrance/exit drives needed. Shared parking proposals must be submitted to the County Planning Department for review and approval. A land use breakdown must be provided with timetables that track the hours each land use is utilized. The appropriate number of stalls then must be appropriated.

Policy 13: Allow reductions in off-street parking requirements where on-street parking is available.

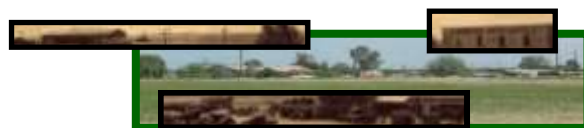
Policy 14: In residential areas, all walls that face public right of way must have landscaping adequate for the full screening of the walls for effective graffiti abatement.

Policy 15: Multi-family residential developments should be organized around common community areas, with parking located to the center or rear of the development.

Objective LU-5: Allow for new development to follow the guiding principles of traditional neighborhood development prior to WWII.

Policy 16: All elements of the built environment must be scaled to the pedestrian, including, but not limited to residential streets, lighting, and store continuity.

Policy 17: Signage should be scaled to the pedestrian.



Policy 18: In residential areas, setbacks must be consistent along public right of way, including development within planned districts (zone districts where setbacks are discretionary).

Policy 19: Residential developments cannot be separated from local streets, park boulevards, or neighborhood collectors with the use of solid walls or fences (excluding Fairmead Blvd. due to high noise impacts).

Policy 20: At least seventy percent of residences in new development should have porches positioned facing parcel frontage.

Policy 21: For garages facing the front of the parcel, garage location may extend up to twenty feet beyond the front façade of the residence. Garages must not dominate street frontage.

Policy 22: Front doors and porches should only face streets.

Policy 23: Proposed window or door positioning cannot violate the privacy of adjacent homes.

Policy 24: All residential parcels are encouraged to take access from rear alleys when available. Rear access creates a residential street environment that is visually pleasing and oriented to the pedestrian.

Objective LU-6: Provide increased housing choices and affordability.

Policy 25: The County shall work with regional housing assistance agencies for provision of affordable housing units in the Fairmead planning area.

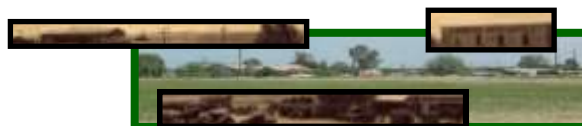
Policy 26: Allow for higher residential densities within the planning area, in addition to conventional low density residential urban development.

- By allowing for higher densities within the planning area, the availability of smaller units will increase along with their affordability. The Fairmead Land Use Plan designates medium density residential, mixed-use/high density, and professional office (includes high density residential only with discretionary approval). These designations allow for lots as small as 2,200 square feet.

Policy 27: The use of second dwelling units (including small apartment add-ons) and minor second detached dwellings is encouraged to increase elderly care opportunities and affordable housing units.

Policy 28: The use of manufactured homes is encouraged to increase housing affordability. Manufactured homes are not exempt from other policies within the Plan affecting residential design.

- Manufactured homes can be designed to reflect many different housing types and architectural styles. Manufactured homes now range from conventional looking detached single family homes to two story townhomes. Most importantly, manufactured homes still prove to be much more affordable than conventional homes, while the



flexibility in type and design has greatly improved. Manufactured homes must, at a minimum, meet the Manufactures Housing Architectural Review (MHA) zone district standards in contributing to high quality, aesthetically pleasing neighborhoods.

Policy 29: The highest priority throughout the entire planning area is the provision of a community sewer system. The lack of a sewer system inhibits the ability to increase residential densities beyond existing levels.

Policy 30: Where community water and/or sewer service is not available, development is allowed with existing services (individual well and/or septic) available at the time and in compliance with all local, State, and federal regulations.

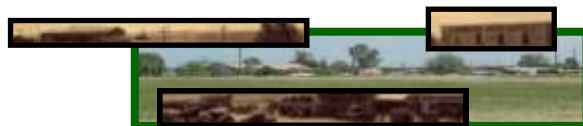


A mix of housing types promoted in medium density areas. Source: Urban Land Institute

Green Design

All new development is to incorporate green design—building design that is energy efficient and environmentally responsible. Such design features may include:

1. Solar power and light: photovoltaic, solar heating, natural lighting.
2. Greywater systems: greywater reuse for landscaping, rainwater catchments.
3. Landscaping installed within public right of way shall consist of drought-tolerant species.
4. The use of water-intensive plants is discouraged.
5. Water conserving fixtures and appliances.
6. Building Orientation: solar orientation, orientation and design to take advantage of natural heating and cooling.
7. Light-colored roofs, paints, and driveway materials shall be encouraged to reduce heat absorption throughout the community and create a cooler environment.
8. Develop residential neighborhoods without fireplaces or with the use of U.S. Environmental Protection Agency certified, low emission natural gas fireplaces.
9. Subdivision Maps, Parcel Maps, Conditional Use Permits, Site Plan Review, and Planned Development Review projects must be evaluated to ensure compliance with air quality standards (as determined by the San Joaquin Valley Air Pollution Control District), including



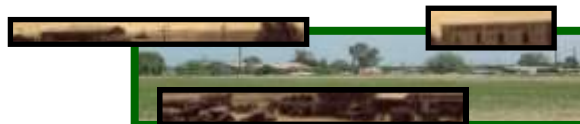
construction, area source, and operational emissions.

10. To contribute towards the reduction of diesel emissions, all commercial dock and delivery areas that accept diesel trucks shall include:
 - a. Signage advising truck drivers to turn off engines when not in use.
 - b. Signage advising truck drivers of State law prohibiting diesel idling for more than five minutes.
 - c. Auxiliary 110 volt and 220 volt power units so trucks can power refrigeration units or other equipment without idling.

Williamson Act Compliance

14 parcels encompassing 1,332 acres currently subject to the Williamson Act are designated for non-agricultural uses in the land use plan. The Area Plan will automatically create inconsistency between the existing contracts and the General Plan.

In order to establish consistency with the Williamson Act, the uses and zones as identified in the Area Plan will not become effective until the subject parcels have fully come out of the Williamson Act program. Subject parcels must have either filed for nonrenewal (and let the contract run out) or have had their contract(s) canceled in compliance with Williamson Act provisions.



CHAPTER ~ 5 ~

Circulation

Circulation Plan Concept

In order to foster a community that is environmentally responsible and healthy for its residents, the circulation system must serve to create walkable neighborhoods and provide a variety of transportation choices. By meeting these needs, air quality impacts can be minimized and residents will be provided healthier living and transportation options.

While the existing community is overlain with an efficient grid network very suitable for pedestrian access, the existing road and sidewalk system is in poor condition. Many Plan policies focus on the upgrade of the entire circulation system, including well-built and maintained travel lanes, sidewalks, and other components such as bike lanes and pathways. Other policies include the need to carry the existing grid network into new developments and a system that emphasizes pedestrian connections over the automobile.

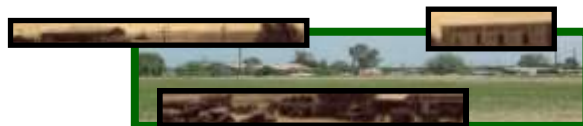
Fairmead lies at a key intersection in the San Joaquin Valley, the junction of California’s major north/south highways (Highway 99) and one of the major east/west routes (Highway 152). With the conceptually planned extension of Highway 152 to the east, the community’s importance at the intersection of two major regional routes will only increase.

Road System Design

The ability to access a regional highway will be very easy throughout the planning area. The connection of Avenue 21 ½ into a new Highway 99 interchange has relocated access to the town farther east and south. Road 20 will become the community’s main north/south arterial. The northern part of the arterial is bowed northwest to draw attention to the village core, then jogs back northeast and north towards eastern Chowchilla. Avenue 22 ½ will continue to be the community’s main east/west connection, designed as a four lane connector with on-street parking to accommodate adjacent commercial businesses.



A grid network provides for continuity throughout the circulation system
Source: www.tfhr.gov



Smaller two lane connectors will also serve to distribute traffic loads, along Fairmead Boulevard, Sinclair Drive, and Avenue 22. Along the planned extension of Highway 152 east of Highway 99, an interchange is planned at what will be Road 21 and Highway 152.

Typical block sizes in the existing community are as small as 300 by 400 feet. By keeping block sizes smaller than the average block in other Valley cities such as Fresno or Clovis (average block is 2,640 by 2,640 feet), overall accessibility is increased for all the modes of transportation. The majority of the roads within the community are designed as residential streets with two lanes. Due to smaller block sizes, traffic is dispersed across a greater number of streets, instead of being funneled onto larger arterials, as is typical of many suburban areas throughout California.

Right of Way Classifications

Highway

This designation is specifically for the future expansion of Highway 152 east of the current 99/152 interchange. Right of way shall be a minimum of 280 feet, with two lanes of travel in each direction. All other Department of Transportation standards apply.

Expressway

Designed as a divided road with a maximum of 3 lanes in each direction. Shoulders shall be ten feet on each side. The design speed is 55 mph. Access shall be limited to collectors, arterials, and highways. Right of way shall be 140 feet minimum.

Town Arterial

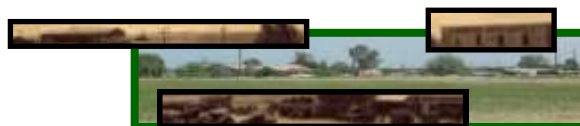
Designed as a divided road with 2 travel lanes in each direction, a 4 foot bike lane on each side, and a landscaped median. The median will be sized to allow for greater green space and simplified maintenance of landscaping and will include turn pockets. This road will have a design speed of 35 mph and have considerable landscape setbacks on each side of the roadway. 96 foot right of way.

Rural Arterial

A road with 2 travel lanes in each direction and a 6 foot bike lane on each side. This road will have a design speed of 50 mph and will be used primarily for connections to adjacent communities. 80 foot right of way.

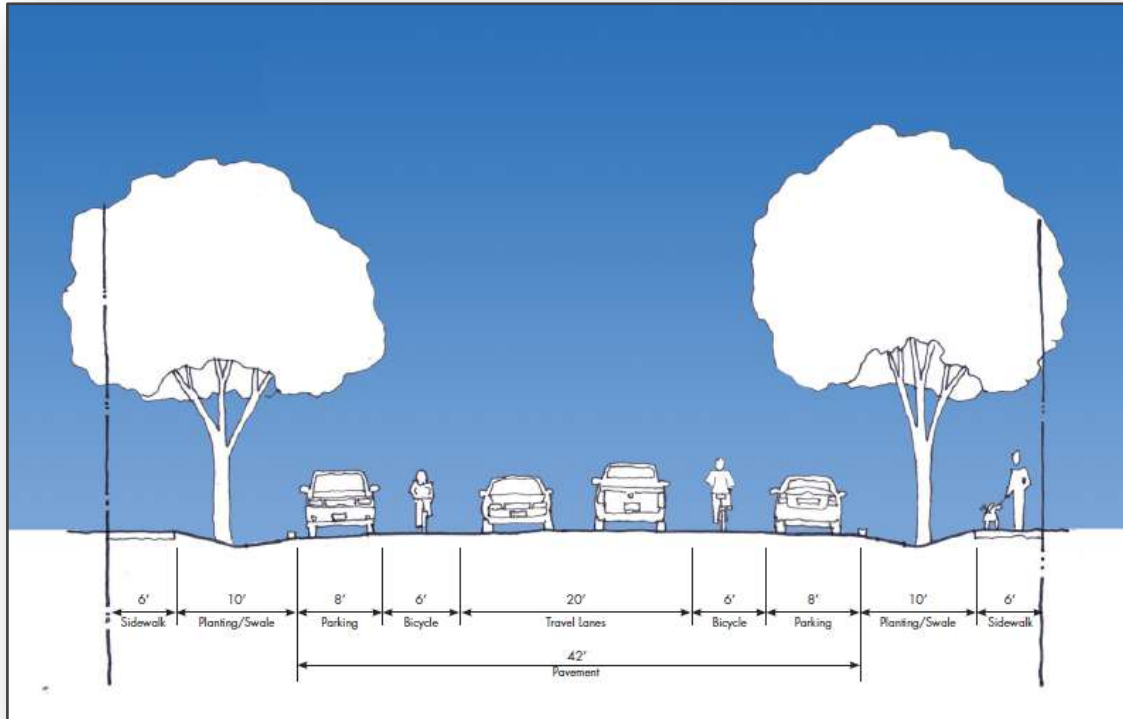
Rural Collector

A road with 2 travel lanes in each direction and a 6 foot bike lane on each side. This road will have a design speed of 50 mph and will be used primarily for connections to adjacent communities. 60 foot right of way.

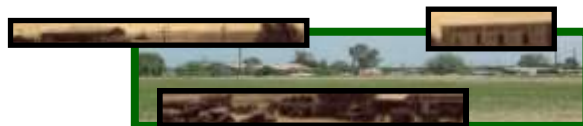


Collector – Avenue 22 ³/₄

Designed as a two lane street with travel in each direction with 42 feet of pavement, including 10 foot travel lanes, six foot bicycle lanes, eight foot parallel parking, a large swale, and six foot sidewalks. The design speed is 35 mph. 80 foot right of way.

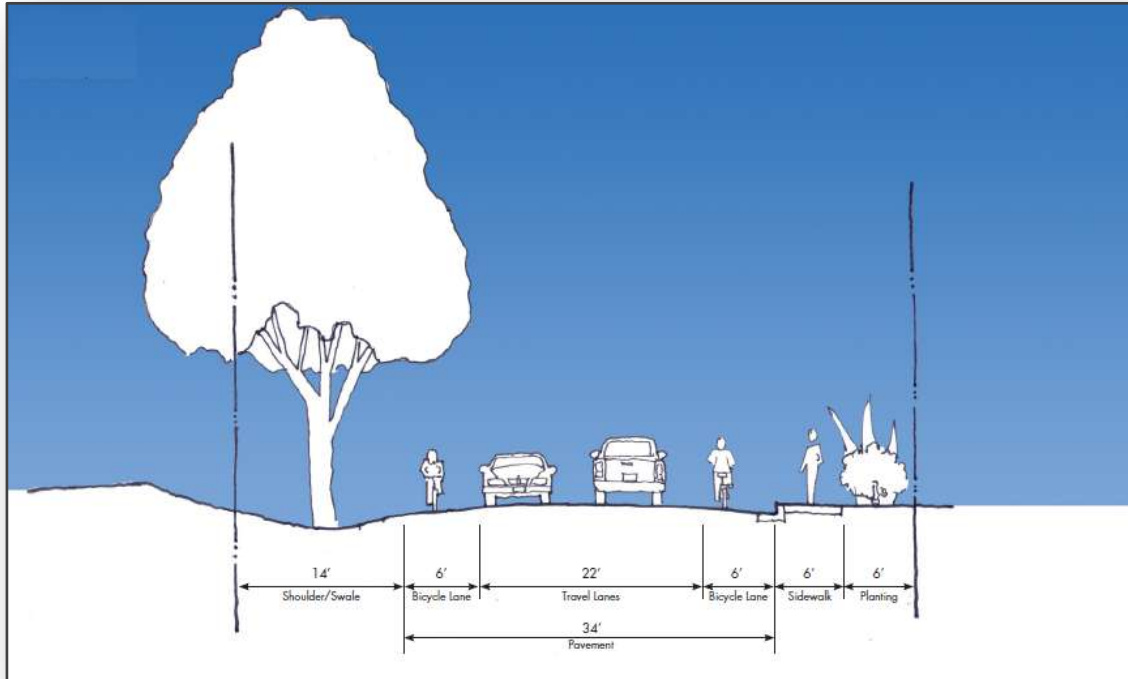


Avenue 22 ³/₄
Source: Opticos Design, Inc.



Collector – Fairmead Boulevard

Designed as a two lane street with travel in each direction, with eleven foot travel lanes, bike lanes, a sidewalk and planting along the east edge (to mitigate air impacts from Highway 99), and a swale with street trees buffering the western edge from the railroad and highway.



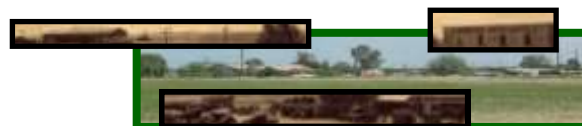
Fairmead Boulevard
Source: Opticos Design, Inc.

Primary Collector

Designed as a two lane road with travel in each direction, a 4 foot wide bike lane on each side, 16 foot wide landscaped median with turn pockets, with parking on each side. Sides shall feature a 5 foot wide landscaping strip and a 5 foot wide sidewalk. The design speed is 40 mph. 88 foot right of way.

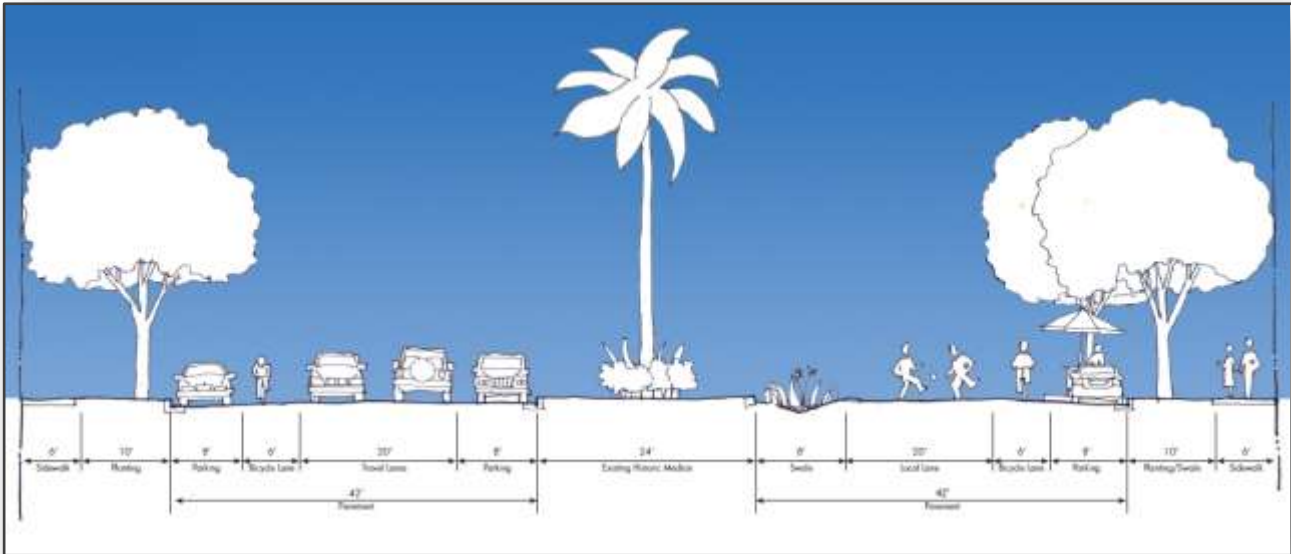
Commercial Street

Designed as a two lane street with one travel lane in each direction, alternated parking on each side, and 15 foot wide sidewalks on each side. Street trees and street lamps will be on each side of the street, along the sidewalks, and immediately abutting the street. The design speed is 25 mph. 70 foot right of way.

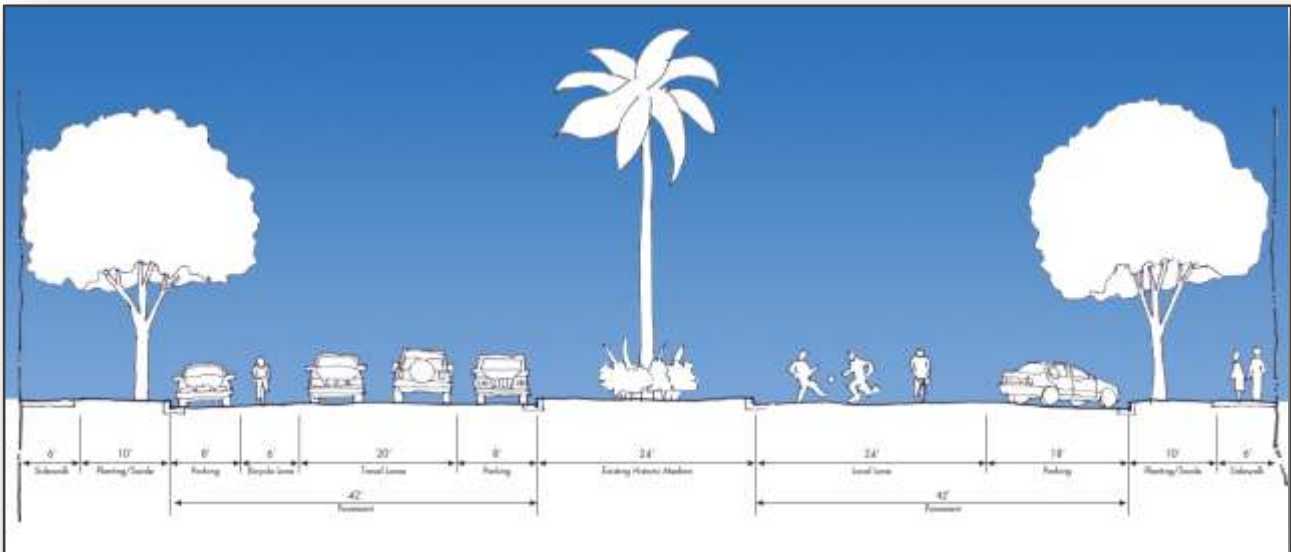


Local Street – Maple Street

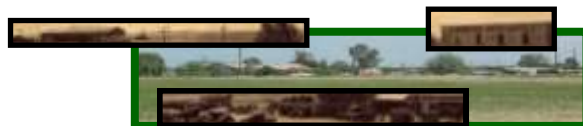
A number of streets in the town core were designed with wide landscaped medians, wide travel lanes, and sidewalks on both sides. Both Sinclair and Maple Avenues remain today with some of the original right of way. Below are a number of design options for Maple Street, which will remain as a local street serving residential traffic.



Maple Street with swale and residence traffic on west side.
Source: Opticos Design, Inc.

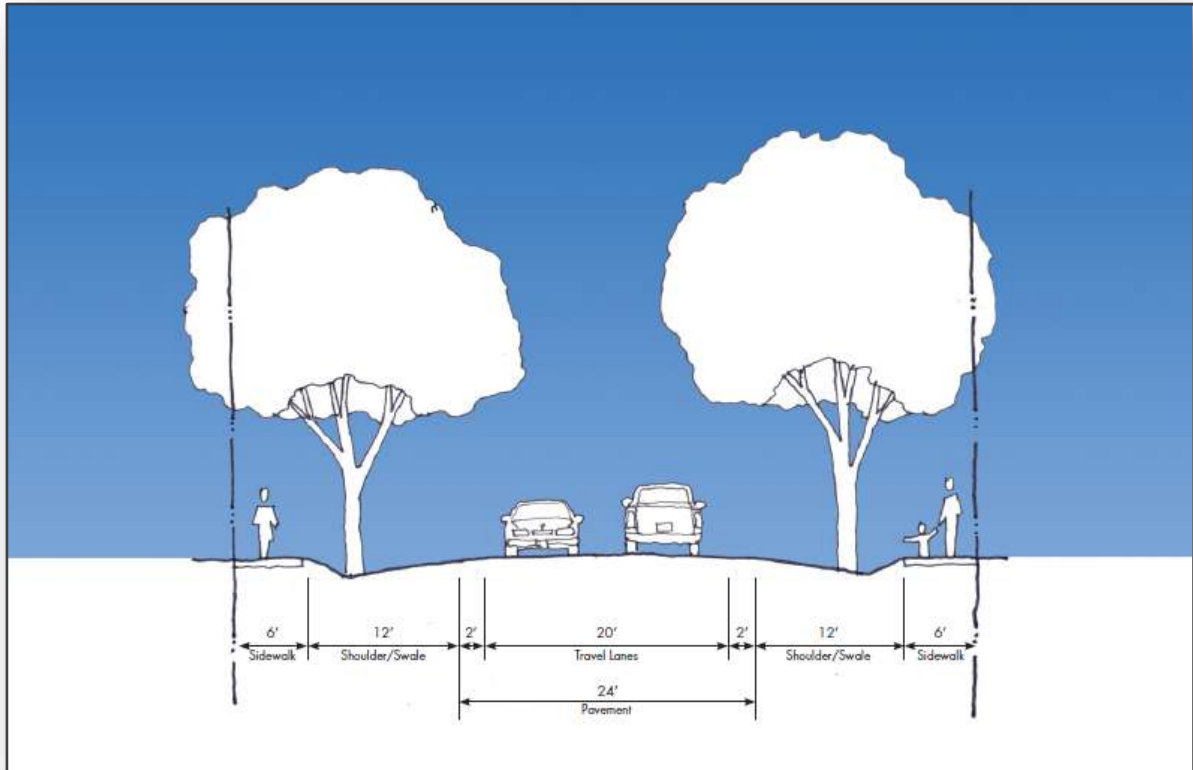


Maple Street with parking and play area on west side.
Source: Opticos Design, Inc.



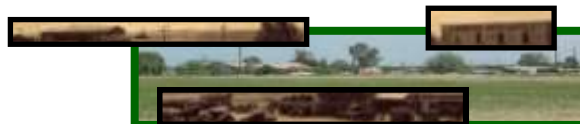
Local Street - Typical, 60 foot Right of Way

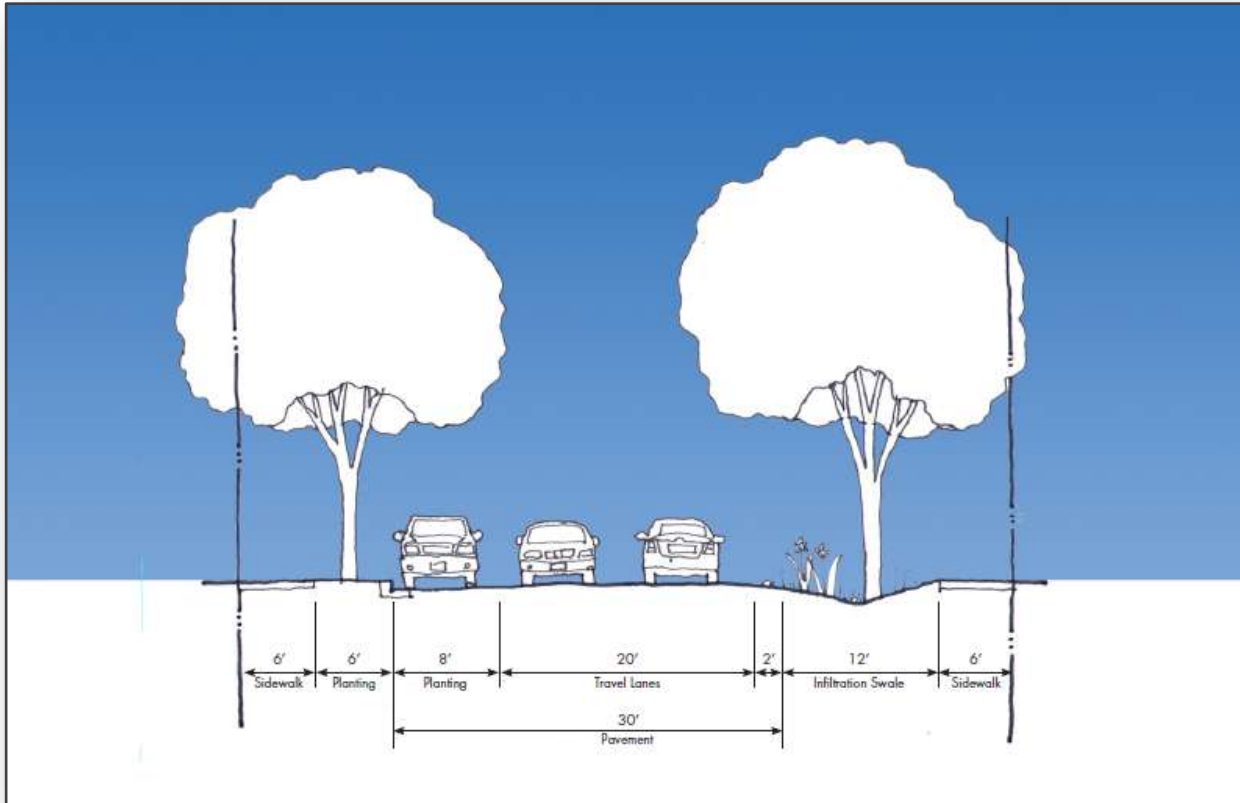
Other local streets in the town core have a sixty foot right of way. These streets include Moore, Yates, Elm, and several others. Two options are provided, each provides two lanes and travel in each direction. Swales are also provided, along with sidewalks.



One option for typical local streets includes two 10 foot travel lanes, swales on both sides and sidewalks.

Source: Opticos Design, Inc.





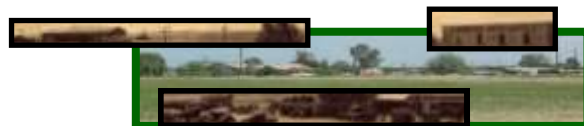
Another option for the typical local street includes two 10 foot travel lanes, a swale on one side and curb and gutter on the other side. Sidewalks are also included.
Source: Opticos Design, Inc.

Residential Alley

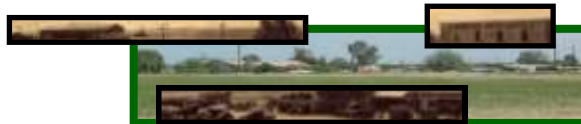
Up to a 26 foot right of way, with a 14 foot travel lane, one foot curbs, and up to 5 foot landscaping strips. Garages may be setback as little as 5 feet from right of way edge and as far as 20 feet removed from right of way edge.

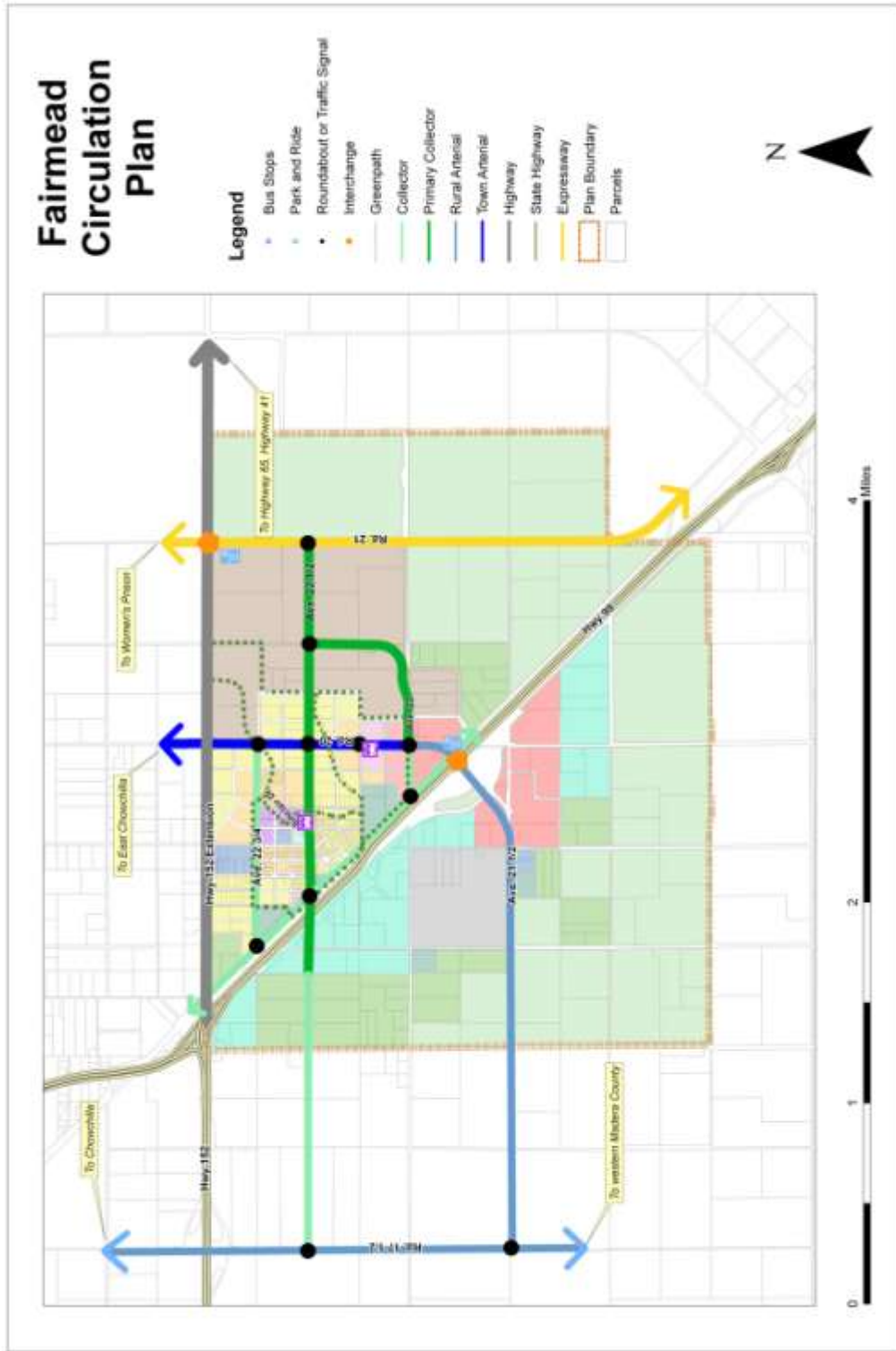
Streets Adjoining School Sites

Up to 60 foot right of way. Streets adjoining school sites shall utilize traffic calming techniques in order to allow slower traffic around school sites. Physical traffic calming measures must be utilized, including bulb-outs (at intersections) and chokers (mid-block crossings). Street design shall allow traffic speeds of no more than 25 miles per hour. Where there are students drop-off and pick-up adjacent to schools, specific street cut-out shall be provided to accommodate busses and private passenger vehicles. Pavement areas must be widened a minimum of fourteen feet to allow for a bus



unloading area alongside the school site. Private vehicle unloading must be physically separated from bus unloading to ensure safety. Pavement area must be widened a minimum of twelve feet to allow for a private unloading alongside the school site. Pavement marking shall identify two twelve foot traffic lanes. Class II bicycle facilities shall abut traffic lanes and shall be fully marked at six foot width, on both sides of the street. Where bicycle lanes cross unloading areas, signage shall caution bicyclists and drivers. Sidewalks shall be ten feet in width and shall not cross unloading areas. Where necessary appropriate street signs, signals, pavement markings, and caution lights, etc. shall be installed to protect students, pedestrians, and bicyclists.





Circulation - Goals, Objectives, and Policies

Goal: Ensure enhanced mobility is provided within the community and to the surrounding region.

Objective C-1: Plan and provide connections from the existing community to the surrounding region.

Policy 1: The County shall work with Caltrans to incorporate gateway monuments and other forms of signage within County and/or Caltrans right of way, marking entrances to the community from multiple directions.

Policy 2: Proposed projects shall avoid planned right of way. Otherwise, projects located abutting planned right of way shall ensure the reasonable continuity of the corridor is met.

Policy 3: Protect areas of planned right of way.

- Studies must first be conducted to determine final right of way locations, based on the Circulation Plan and the Regional Transportation Plan.
- Interim uses allowed within planned right of way include: outdoor recreational facilities, produce stands and farmers markets, periodic events such as festivals or community fairs, agricultural uses, and outdoor advertising.
- Proposed projects shall avoid planned right of way. Otherwise, developments proposed adjacent to or abutting planned right of way shall ensure that the reasonable continuity of the corridor is met.
- Where right of way currently exists, proposed projects must dedicate and improve their portion of said right of way as a condition of project approval.

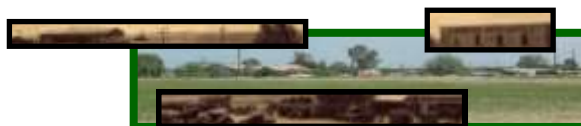
Objective C-2: Provide smooth, efficient internal connections within the community.

Policy 4: Consider roundabouts and traffic circles to facilitate increased mobility and reduced air quality impacts within the planning area.

Objective C-3: Ensure that project-specific impacts upon the transportation system are alleviated by the developer.

Policy 5: More specific project impacts will be analyzed in proportion to each project's impacts to the planning area's circulation system and impacts to the state highway system. Project specific mitigations will then be incorporated as appropriate.

Policy 6: Project specific impacts relating to adjacent state facilities will be assessed and must comply with all necessary mitigation measures as addressed by the Department of Transportation.



Policy 7: Project specific impacts relating to the County road system will be assessed and must comply with all necessary mitigation measures as addressed by the County Road Department. All mitigations must be in compliance with Area Plan Circulation standards.

Goal: Create a community that provides for all modes of transportation.

Objective C-4: Create a walkable pedestrian environment.

Policy 8: Create a pedestrian system of sidewalks and recreational paths for both accessibility and recreational needs.

Objective C-5: Increase mass transit availability and access.

Policy 9: Bus stops shall be developed at the locations identified on the circulation plan. Funding is to be provided through coordination between the Madera Action Partnership, Madera County Transportation Commission, and adjacent project developers (including private monies, grant funding, sales tax funds, and other possible funds).

Policy 10: Develop park and ride facilities as indicated by the circulation plan. Public-private partnerships are encouraged where funding may be provided through the efforts of the Madera Action Partnership, Madera County Transportation Commission, and adjacent project developers (including private monies, grant funding, sales tax funds, and other possible funds).

Objective C-6: Provide for a bicycle-friendly environment.

Policy 11: Allow room for bicycles on improved streets. Where right of way is available, provide Class II bike lanes.

Policy 12: Ensure that new recreational paths are designed as multi-use, bicycle and pedestrian friendly facilities.

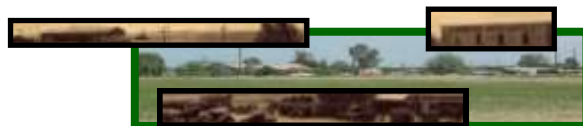
Policy 13: All multi-family residential, commercial, industrial, and institutional projects shall comply with the following: (1) bicycle rack able to accommodate at least (1) bike shall be provided per 0.5 parking stalls as required per County Code 18.102.040.

Goal: Create a safer environment for all modes of transportation.

Objective C-7: Ensure safety through community design.

Policy 14: Only right-in, right-out movements are permitted at the intersection of the following: 1) town arterial and all local streets and alleys and 2) primary collectors and all local streets and alleys.

Policy 15: Utilize landscaped medians to facilitate right-in and right-out traffic movements from local streets and alleys onto town arterials and primary collectors.



Policy 16: Roundabouts must be individually designed given the number of intersecting streets, street widths, and intersection angle.

Policy 17: Roundabouts must not be designed for over town land (capable of accommodating four lane streets).

Policy 18: Regarding roundabouts, splitter islands (as defined within Chapter 5) must be utilized to facilitate safe turning movements and pedestrian safety.

Policy 19: Utilize traffic calming devices throughout the community to encourage pedestrian accessibility and mobility.

Objective C-8: Circulation and transportation systems within the Fairmead Community shall provide for the safety and hazard free movement of students from homes to schools.

Policy 20: Public right of way improvements adjacent to school sites shall contain bus and passenger vehicle turnout or frontage lanes to provide safe and hazard free passenger drop-off and pick up.

Policy 21: Student drop-off and pick-up location shall be provided within the community. Such locations shall provide for bus turnout or frontage lanes, and protective shelters with seating.

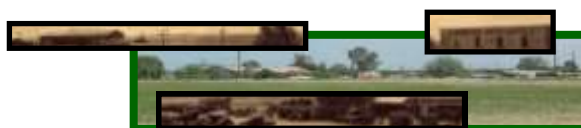
Policy 22: Appropriate traffic signalization, caution signals, and signage shall be installed in accordance with the requirements of the Chowchilla Elementary School District and the Chowchilla Union High School District based on primary pedestrian and bicycle routes of students from home to school to minimize and/or provide further safety for crossing guard services.

Policy 23: If utilized in close proximity to school facilities, roundabouts, traffic circles, and traffic calming devices shall be designed to accommodate the bus sizes contemplated and required for use by the Chowchilla Elementary School District and the Chowchilla Union High School District.

Policy 24: The County shall strive with the Chowchilla Elementary School District and the Chowchilla Union High School District in developing a program for street, highway, railroad, and flood control crossings and safe student pedestrian and bicycle routes to schools.

Policy 25: The County shall consult with the Chowchilla Elementary School District and the Chowchilla Union High School District in developing and implementing school bussing routes that are cost effective, safe, and efficient.

Policy 26: The County shall support child care facilities located within transportation hubs by supporting research on the feasibility of locating child care centers at "Park and Ride" sites, transit centers or other locations accessible to public transportation.



Objective C-9: Ensure that streets abutting schools include speed controls and calming devices that provide a safe environment for school children.

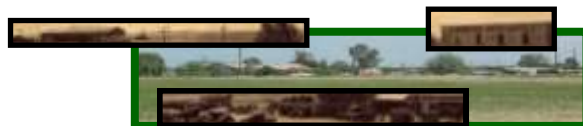
Policy 27: All streets abutting school sites (existing and planned) with direct access to the site shall have school safety zones with limited speed limits.

Policy 28: All streets abutting school sites shall consider traffic calming techniques that will effectively slow traffic to allow a safe pedestrian environment. Traffic calming devices are described on the following page.

Objective C-10: New private and public development projects must mitigate both construction and operational impacts upon schools, school district transportation routing and schedules, and sensitive receptors that may be impacted by development.

Policy 29: Projects must comply with the San Joaquin Valley Air Pollution Control District's Guidelines for Assessing and Mitigating Air Quality Impacts (GMAQI) as applicable to sensitive receptors.

Policy 30: The construction of all streets, roads, circulation and transportation improvements within the Fairmead area shall be in compliance with the mitigation measures and requirements of the Madera County Transportation Commission as applicable to schools, school district transportation routing and schedules, and sensitive receptors, including but not limited to schools, as set forth in the 2011 Regional Transportation Plan Environmental Impact Report.



Important Elements of the Circulation System



Single lane roundabout designed for sharp-angled intersection.
Source: RBF Consulting

Roundabouts

Roundabouts allow for smooth and efficient traffic flow and a mitigation device to alleviate air quality impacts. Roundabouts are designed to efficiently deal with intersections with three to four streets converging at sharp angles or streets that converge offset from one another.

Traffic Circles

Traffic circles are encouraged at the intersection of local streets to enhance pedestrian safety. Traffic circles must be landscaped to enhance neighborhood aesthetics.

Traffic Calming Devices

The use of traffic calming measures is encouraged throughout the planning area. Such measures will reduce traffic speeds and heighten pedestrian awareness. Measures must be implemented in close proximity to public areas. The two general types of measures include:

Horizontal

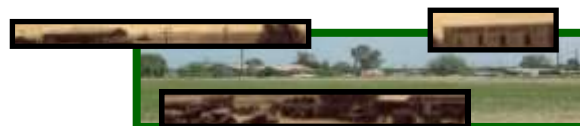
- Traffic Circles
- Bulbouts (*intersection corners protrude out*)
- Chokers (*protrusions at crosswalk points*)

Vertical

- Textured pavements
- Raised crosswalks
- Raised intersection

Bike System

This Plan is designed to achieve the goals and policies of the Madera County General Plan as related to bicycle transportation (General Plan Goal 2.D). The bicycle system serving Fairmead and the surrounding area is designed to provide the greatest amount of access to the area and will provide recreational opportunity. Bicycle facilities will include Class I paths, Class II paths, and Class III paths.

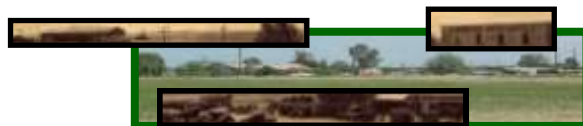


- Class I Paths are 10' wide and paved, running along one side of the street. These paths run the length of the streets designated as a residential boulevard.
- Class II paths are immediately adjacent to auto traffic lanes (both ways), are painted and identified, and are 6' wide. Streets designated as arterials and collectors will have Class II paths.
- Class III paths are immediately adjacent to auto traffic lanes (both ways), yet they are not painted and identified. Roads are built wide enough for bikes to stay out of traffic lanes. Streets not identified as having Class I or II paths feature Class III paths.



Pedestrian Access

All new neighborhoods must emphasize pedestrian access over all other forms of transportation. The majority of the residential neighborhoods surrounding both district centers are within ¼ mile of the center. One quarter mile is widely considered to be a convenient distance that most people will be



willing to walk, before they consider automobile use. Park space is also spread throughout the community to promote pedestrian accessibility.

- Large sidewalks (15 feet) along commercial streets within the village core allow for ample pedestrian space. All other streets are required to install sidewalks when upgraded.
- Crosswalk options include the following:
 - A. Textured and raised
 - B. Textured
 - C. Painted to appear textured



A multi-use path with ample landscaping
Source: ksgaccman.harvard.edu

Green Paths

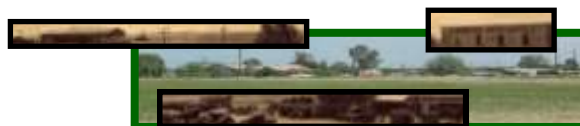
The green path system incorporates both pedestrian and bicycle use onto a multi-use path—much like recreational trails seen in many new developments or “boardwalks” seen in many California beach towns. The “green” in the path describes another function of the path system as greenbelts, which provide a heavily landscaped edge against agricultural areas and a buffer along community drainage channels. The green path system was designed into the concept plan to maximize pedestrian and bicycle access apart from the road network. The system also provides for pedestrian and bicycle interconnectivity throughout both the Gateway and Fairmead Township Districts.

Transit Facilities

Mass transit is a vital part of this Plan as a transportation mode that allows disabled, low cost, and regional access. For many low income groups, transit may be the best choice if a car is not available or rising fuel costs prompt drivers to seek alternative modes. Currently, the Madera County Connection provides bus service to the community. The Planning Department will work with the Madera County Transportation Commission to ensure that this service remains intact.

Circulation Related to Schools

Circulation systems in the community shall address the particular needs related to the development and operation of schools facilities within the Fairmead Community. Policies pertaining to circulation and schools are addressed in Chapter 6 under Objective CFS-6.



CHAPTER ~ 6 ~

Community Facilities, Services, & Infrastructure

Community facilities, services, and infrastructure within the planning area must be provided on a comprehensive basis. The current lack of facilities in the area is a Plan priority that must be addressed before the initiation of development or at least as development occurs.

Education

Two important educational institutions that operate within the existing community include the Headstart Program and the Chowchilla Elementary School District at Fairmead Elementary. While a high school is not located in the community, the Chowchilla Union High School District extends into the planning area.

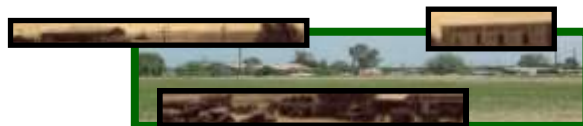
At build-out, the proposed population would require at least one K-5 elementary school, in addition to the existing Fairmead Elementary. The Chowchilla Elementary School District envisions the existing Fairmead Elementary school site will remain and evolve into a K-5 elementary school. The planning area can also support a middle school and high school, if augmented by populations adjacent to the planning area (most likely from the City of Chowchilla environs). To provide for the maximum educational fulfillment and opportunity of the planning area, three “floating” school sites are included for a K-5 elementary school, middle school, and a high school. The State has established the following school sizes as acceptable:

School Type	Land Area (Acres)
K-5 Elementary School	15
Middle School (grades 6-8)	30
High School (grades 9-12)	55

Source: School District’s established sizes based on State guidelines, Community Systems Associates, Inc.

The land use plan on page 27 shows the floating schools sites as requested by the local school districts.

It is important to note that there will be students served by schools within the Fairmead area that will come from other areas surrounding the Fairmead community that may live on farms and ranches outside the boundary of the Fairmead Area Plan. As the City of Chowchilla develops in a southerly



direction along Highway 99 towards Fairmead and the City of Madera develops in a northerly direction along Highway 99 towards Fairmead, the Districts will experience student generations that are more conducive to being transported to schools in the Fairmead area versus being transported to schools within the City of Chowchilla which are already at capacity or which are of a quality that do not serve further expansion and utilization. Therefore, the future schools within the Fairmead area are critical to the future of both Districts' capital plan.

Fire Services



A fire station of adequate size for a community of approximately 5,000-7,000 people. Source: HDA, Inc.

The community is currently in need of a fire station, the demand of which will only increase as the community grows. At build-out, the community will need a new fire station able to house twelve people. The 6,000 square foot station recently constructed in Oakhurst is of an acceptable size—a station designed for more of an urban environment and a community of approximately seven thousand people, close to the population of the planning area at build-out. The fire station will be located within the village core, for optimum access to the rest of the planning area.

Law Enforcement

A critical need has been identified in Fairmead for increased law enforcement. Drug problems have been identified by the community as a critical problem. As the community's population grows, the need for the Sheriff's services will only increase.

Emergency Services

The high amount of elderly within the existing town highlights the existing need for emergency services. The small size of the township does not support ambulance service stationed within the existing township. As the planning area approaches build-out, however, there will be a need for an ambulance stationed in the planning area for quick response.

Parks

The provision of park space and recreational activities was noted as a high priority by the residents of the community. Recently, the requirement for park dedication or payment of fees in lieu of has been established and is being implemented through County Ordinance, Title 15. Requirements include the dedication of three acres of park space per one thousand people.

While new development will be required to dedicate or pay fees towards parks, the existing community remains with a strong need for recreational space. The proposed drainage system provides for two retention basins within the existing community—basins which will also serve as recreational areas. Only during large storms will these recreational areas be inaccessible. The largest basin, located on the east side of Fairmead Blvd. and south of Ave. 22 ¼, will offer approximately nine acres of recreational space. Facilities may include a baseball field, soccer field, or other similar facilities.

Community Gathering

Both community workshops identified the need for a facility available to the entire community, including a facility that allows for community activities, learning opportunities for all ages, and an outlet for children and young adults after school hours. A number of possible community facilities could provide such benefits, including a community center or library.

Fossil Discovery Center

An interactive paleontology museum is now located adjacent to the Fairmead Landfill, on the northeast corner of Avenue 21 ½ and Road 19 ½. The landfill contains significant fossil remains dating back to the Pleistocene epoch. The Center is located adjacent to the landfill in order to facilitate interactive tours of the fossil dig within the landfill.

Provision of Services

The County currently operates Maintenance District 33 within the majority of the existing community. Services have recently been improved with the addition of a water storage tank. There is a need in the community for a service provider that has the ability to provide a multitude of services, much like a city, as opposed to a limited maintenance district. There is a need for a service provider that can cover all community facilities, services, and infrastructure systems in Fairmead, including water, future sewer, lighting, landscaping, drainage, roads, fire protection, and any other important services or facilities the community may need. A Community Services District (CSD) is an entity that can cover such a great number of services. The intent of a CSD is



A community park with a playground and ball field.



to give an alternative method for providing governmental services by counties within unincorporated area—an alternative apart from incorporation into a municipality.

Water Supply

The existing Maintenance District 33 water system must be supplemented when any new development is added to the planning area as designated in this Plan. Build-out of the planning area will require very intensive improvements to the existing system. Any new development within the planning area must install a community well to serve the proposed development and supplement the existing system.

Sewer System

In order to achieve the Plan's Vision as a new community in the 21st Century, Fairmead must be equipped with a sewer treatment system that is advanced and environmentally sensitive. A tertiary treatment system can treat water to standards above those required of tap water. This treated water can then prove beneficial to the surrounding area for agricultural reuse and for landscaping irrigation throughout the planning area. Any excess clean water can also be placed in retention basins for groundwater recharge purposes.

A temporary treatment plant may be utilized as the community begins to grow. As the planning area reaches build-out, the treatment plant may be relocated on a permanent basis west of Highway 99, where associated negative impacts to residential areas can be minimized, including strong odors. A sewer system must be included within a future CSD.

Lighting

Both workshops identified a need for lighting in public right of way. While all new development will be required to provide lighting, the existing community must provide lighting through other means. Lighting in existing areas may be funded through grants, reimbursement agreements, or even development agreements. A grant of special importance to Fairmead includes Safe Routes to School (funded through the State Department of Transportation), which could include lighting as a means of providing school children a safe route to Fairmead Elementary.

Lighting design must reflect the overall community design of the specific area. All lighting, including free standing and wall-mounted lights must be scaled to the surrounding uses. For example, free standing lights within the village core must be scaled to the pedestrian to enhance and make the pedestrian experience more comfortable.



Street lighting scaled to the pedestrian. Source: Lumec

Landscaping

Beautification is just one benefit of developing landscaping throughout a community’s public right of way. Landscaping also provides valuable screening to mitigate aesthetic impacts and alleviate air quality issues such as pesticide drift. Landscaping is also an important part of improving the outdoor environment, playing a vital role in cooling and improving air quality. The Circulation Chapter outlines landscaping required along improved roads within the planning area.

Solid Waste

In the planning area, solid waste is collected by Madera Disposal Systems. Waste is transported to the County landfill, located on the northwest corner of Road 19 ½ and Avenue 21 ½, within the planning area. Recycling is not currently provided in the area. However, recycling should be extended to the area as funding allows.

Community Facilities, Services, & Infrastructure – Goals, Objectives, and Policies

Goal: Safeguard both environmental and public health through the provision of community facilities and services that adequately serve the entire community concurrent with the needs of such community facilities and services.

Objective CFS-1: Ensure law enforcement presence is increased as development occurs.

Policy 1: Coordinate with the Sheriff’s office for the long term provision of law enforcement services in the planning area.

Objective CFS-2: Ensure emergency services are increased as development occurs.

Policy 2: Provide for the development of a community fire station within or adjacent to the village core.

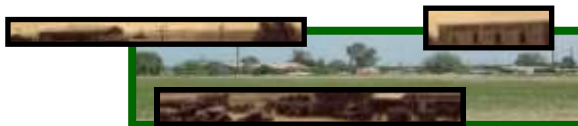
Policy 3: Ensure the fire station is equipped to house ambulance services.

Objective CFS-3: Create a cost-efficient community sewer system available to the entire community.

Policy 4: Acquire land suitable for both a short-term sewer facility and a long-term facility. The short-term facility shall be located on the east side of Highway 99 as identified within the land use plan, serving the more immediate needs of the existing community. Land may be designated for the long-term facility in the southwestern corner of the planning area. New sewer treatment facilities must treat to a tertiary level.

Policy 5: Seek low-cost sewer hook-up assistance for all existing residents.

Policy 6: When made available, require that reclaimed water is used for all landscaping within public property. All new developments are required to utilize reclaimed water for landscaping purposes when available.



Policy 7: Reclaimed water should be available for agricultural use in the planning area. The County shall coordinate with the Madera Irrigation District and the Chowchilla Water District regarding reuse for agricultural purposes.

Policy 8: All existing individual septic system must be destroyed and removed upon construction of a new community sewer system.

Objective CFS-4: Enlarge and develop the existing water system to provide a sufficient supply of clean water to the entire Area, while ensuring efficient, conservative water use.

Policy 9: Enlarge the existing water system to include the entire existing community and require that every new development connects to the system.

Policy 10: Continue seeking grant funds to support the development of the existing system.

Policy 11: Project applicants shall coordinate with the Chowchilla Water District and Madera Irrigation District for potential use of surface water supplies and help contribute towards a one-to-one water balance. Any potential use of water/irrigation district supplies is subject to approval by the Local Agency Formation Commission (LAFCO).

Policy 12: New development is required to have water-conserving design and landscaping, including drought resistant landscaping and low-water use equipment.

Policy 13: To further address Valley aquifer overdraft, each development will be required to establish a one-to-one water balance. In other words, for every gallon of groundwater utilized by the subject development, one gallon is required to be recharged back into the aquifer.

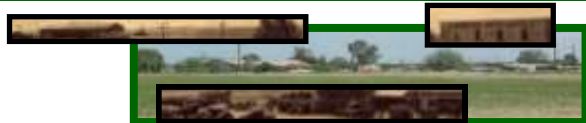
Objective CFS-5: Support and reinvest in existing community facilities, services, and infrastructure systems that the community can rely upon.

Policy 14: Maintain the highest possible level of educational services, school facilities, and educational programs, including quality child care options for all Fairmead community residents in conformance with the policies, standards, rules, regulations, and requirements of the Chowchilla Elementary School District and the Chowchilla Union High School District.

Policy 15: Create financing mechanisms that will support the short term and long term viability of permanent and interim community facilities and services in Fairmead, as defined in the Area Plan.

Policy 16: Support the reorganization of MD-33 into a Community Services District (CSD) for the inclusion of multiple services as necessary.

Policy 17: Ultimately, the new services district should include at least water, sewer, lighting, roads, parks, drainage/flood, and landscaping.



Policy 18: All new development within the planning area must annex into the service district existing at the time of project approval to assist in augmenting the district's service capabilities.

Objective CFS-6: Provide public gathering areas and recreational spaces that serve to knit the community together.

Policy 19: Establish and maintain a system of public parks and other recreational areas within the community.

Policy 20: Establish and maintain a community center strategically located within the community.

Policy 21: Joint use facilities and other joint use opportunities should be considered in conjunction with the Chowchilla Elementary School District and Chowchilla Union High School District. Joint use facilities may include school open space and fields, park space or a library. Joint use of facilities shall be subject to joint use agreements and shall consider the capital cost of facilities, the ongoing cost of operations and maintenance, and the use shall not conflict with the normal use and operations of the facilities.

Policy 22: Integrate recreational space into drainage basins as identified by the Drainage Master Plan.

Policy 23: Development along green paths should locate parks along the green paths to maximize access and recreational opportunities. Otherwise, parks should be located at easy to access and easy to locate areas, including intersections or community nodes.

Policy 24: Design large residential projects to have open common areas where the community may interact.

Policy 25: Develop a community park at the heart of the town center that provides a centralized gathering place for the entire community.

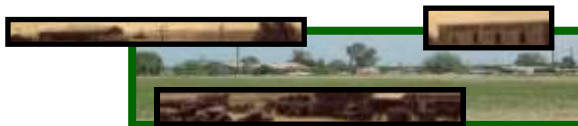
Objective CFS-7: Ensure solid waste produced in the planning area is recycled to the extent feasible.

Policy 26: The County shall ensure that recycling is available to properties within the Planning Area as funding allows.

Policy 27: All construction waste associated with implementation of the proposed Plan shall be recycled to the extent feasible.

School Site and Facilities

Objective CSF-8: Strive for school sites and facilities to be an integral component of the community. School sites and facilities will be the educational center of the community and a central location for other community activities, including recreational activities, and through design and layout, they will enhance the quality of neighborhoods.



Policy 28: School Facilities shall be an integral component of residential neighborhoods where possible so as to provide for efficient pedestrian and vehicle access/egress and to minimize conflict with inappropriate uses.

Policy 29: The design and layout of school facilities shall be in accordance with the requirements and standards of the Chowchilla Elementary School District and the Chowchilla Union High School District Facilities Master Plan and any other policies, standards, rules, regulations and requirements, including but not limited to net acreage requirements, ratio of width to depth, number of sides of the school facility adjacent to roadways, bus drop-off/pick-up frontage lanes, energy efficiency, value engineering, and driveway access/egress.

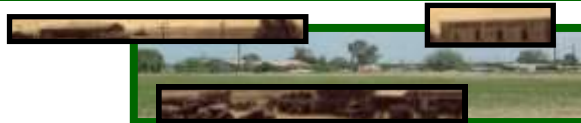
Policy 30: Elementary and middle schools shall be encouraged to be located and designed to be compatible with residential neighborhoods in order to foster the concept of neighborhood schools, to minimize bussing of students, and to encourage neighborhood identity. High schools shall be encouraged to be located and designed to be compatible with surrounding land uses.

Policy 31: The site planning and design of developments which include school sites within or within proximity shall provide efficient and effective access/egress to such school sites, all utilities including, but not limited to gas, electricity, water, sewer, storm drainage, telephone, and cable systems to the property line of such school sites, and related off-site improvements including street improvements, street lighting, sidewalks, parkways, landscaping, curbs, gutters, driveway curb cuts, and driveways from streets adjoining the properties. In the event the school site is located adjacent to a land use other than open space, a block wall shall be constructed on the adjoining property line.

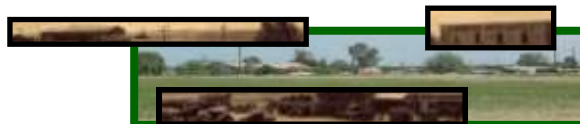
Policy 32: School facilities shall be designed and constructed of materials that are compatible with the designs and materials of adjoining developments provided that such designs and materials are in conformance with the adopted policies, standards, rules, regulations, and requirements of the Chowchilla Elementary School District and the Chowchilla Union High School District.

Policy 33: The County shall consult with the Chowchilla Elementary School District and the Chowchilla Union High School District in the siting and design of school sites in order to implement public transportation, private transportation, pedestrian, and bicycle transportation routes which promote safe and hazard free access/egress to school sites.

Policy 34: The Area Plan provides for the possible development of three 'floating' school sites for future Fairmead school facilities, including but not limited to one (1) elementary school, one (1) middle school, and one (1) high school or any combination thereof. Additional schools may be required in conjunction with the approval of development within the Urban Reserve areas of the Area Plan. All preliminary application documents for any subdivision map, conditional use permit, site plan review, rezone, or general plan amendment, shall be sent to the Chowchilla Elementary School District and the Chowchilla High School District to obtain a report from the applicable school district(s) to determine if any portion or all of the property is required by the school district(s) for a school site(s) within the Area Plan area. In the event that any portion or all of the property is required for a school site(s), the County shall consult with the applicable



school district(s) to allow for the acquisition, reservation, and/ or dedication of the applicable school site(s), or allow a site that the applicable district has identified as an alternative school site(s) within or outside the Area Plan area that will serve the interests of the district and the community and the applicable school district has released the subject floating school site. The school districts have 30 calendar days from receipt of the preliminary application documents to furnish the report to the Planning Department.



CHAPTER ~ 7 ~

Drainage

In order to allow any significant growth and revitalization within the planning area, the flood zone that dominates the southeastern portions must be controlled to prevent any future flooding within the existing community and areas planned for growth.

Drainage System

In order to allow for compact growth adjacent to the existing community, the Land Use Plan accommodates two drainage channels through the flood zone, running along low lying areas as identified through site visits. Both of these drainage channels can be directed to flow into a retention basin at the lowest spot of the flood zone on the eastern side of Highway 99.

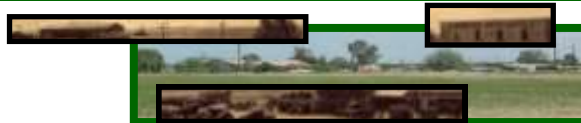
This retention basin is the largest designed in the planning area and is located on the south side of Avenue 22 ¼. Named Basin B, the basin handles runoff from the majority of the planning area on the east side of Highway 99. Immediately to the north of Basin B is Basin A, which handles runoff from the northwest corner of the community—covering a large portion of the existing community. The topography slopes slightly northwest in this area rather than southwest, thus warranting the basin. A connector line will connect both Basin A and B, so both basins can relieve and balance out any excess volume.

Basins are also planned for the planning area west of Highway 99. The lowest point is located immediately east of the landfill, thus Basin C will be located in this area. Basin D will be located adjacent to the landfill to the south to alleviate runoff from the south-central section of the planning area. Both basins C and D were located adjacent to the landfill for the possible use of removed dirt for capping in the landfill.

Apart from the obvious use of retention basins to alleviate drainage issues, they are also effective in aiding groundwater recharge and providing possible recreational space. Both Basin A and B, being located within the community, will provide extensive community park space. Basins C and D



Retention basin also utilized as a soccer field



are located in an area with no residential uses, thus both basins will not feature any recreational use.

Drainage – Goals, Objectives, and Policies

Goal: Solve the community’s drainage and flood problems through the provision of a system that provides multiple benefits to the community.

Objective D-1: Protect existing drainage areas for the capture and treatment of runoff and limit areas subject to flooding.

Policy 1: Drainage channels require at least 70 foot easements to allow for stormwater flow, landscaping, and multi-use paths. All buildings abutting drainage easements must be setback at least 25 feet from easement edge to allow ample sunlight into the drainage area. Drainage channels must be at least 20 feet wide and banked. A 25 foot landscaped zone setback is required along the drainage channel on both sides to accommodate the green path multi-use path. The landscaped zone also serves to handle increased runoff during heavy rain events.

Policy 2: Within the drainage easement, at least thirteen feet should be set aside for use as a multiuse path.

Policy 3: Drainage easements must be dedicated upon development of the parcels that swales bisect.

Policy 4: Integrate the stream system into the overall Master Drainage Plan. Utilize drainage channels as main storm drain lines for the planning area.

Policy 5: When right of way is improved, develop bio-swales in conjunction with the curb and gutter system for the treatment and control of stormwater.

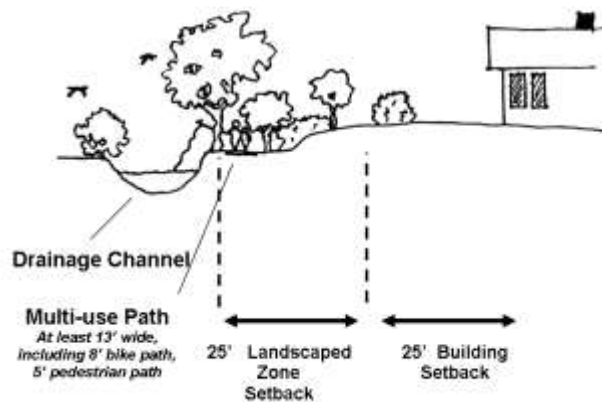
Policy 6: Where right of way or property is available, develop rain gardens to collect, absorb, and slow stormwater flows.

Objective D-2: Ensure the timely development of the drainage system

Policy 7: Planned drainage systems shall be improved as development occurs and as other sources of funding allows.

Policy 8: The County shall work with developers to ease drainage improvement costs, through grant funding and other possible means.

Policy 9: Ongoing maintenance of the drainage system shall be managed and funded through a CSD.



Objective D-3: Ensure that the development of the drainage system complies with existing flood and wetland regulation.

Policy 10: The master drainage plan must incorporate discussion and evaluation regarding consistency with Section 404 of the Clean Water Act.

Policy 11: The master drainage plan must include a wetland delineation. Consultation is required with a qualified biologist.

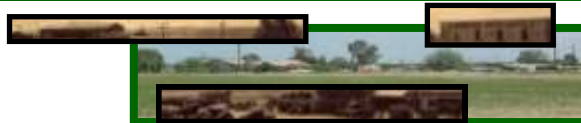
Policy 12: The County Engineering Department will conduct a Letter of Map Revision (LOMAR) through the Federal Emergency Management Agency (FEMA) to the National Flood Insurance Program (NFIP). The end result of the effort will be to establish potential flood elevations and flows throughout the flood zone.



*A rain garden utilized to collect stormwater in a commercial area.
Source: Opticos Design, Inc.*



*A swale utilized along a residential /local street.
Source: Opticos Design, Inc..*



CHAPTER ~ 8 ~

Financing & Implementation

Integral to the revitalization or improvement of any area is a financing plan that identifies all possible avenues of acquiring funds to pay for community improvement projects. All of the basic components of the community’s infrastructure system and services need funding sources identified. Many community projects will require funds from multiple sources be brought together to fund larger projects. Other improvements can be made in increments as development goes in or as grants and other funds can be acquired.

Existing Funding

Funds for Maintenance District 33 are currently acquired through assessment taxes from the existing property owners within the district. These funds pay for maintenance of the community water system. Unfortunately, funds have never been substantial enough to maintain the water system over its lifespan.

Both Chowchilla Elementary School District and Chowchilla Union High School District represent that at the current time, financial resources are not available to expand or provide for new facilities to serve growth as planned within the Area Plan.

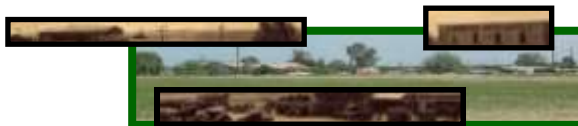
Additional Funding Options

Various funding options are available; however, most funding sources can only be directed to a specific use and are severely limited in the amount available. For this reason, it is important to mix and match funds as needed for larger projects. Possible funding sources include:

1. Project Developers

Project impact and mitigation funding, along with other tools such as reimbursement agreements are important for preparing an area for the associated impacts of development and improving the area’s overall infrastructure and services.

- a. *Subdivision Map*: Depending on the project size, type, and location, the Subdivision Ordinance (Title 17), requires various improvements to be made in conjunction with and directly related to a specific development.
- b. *Reimbursement Agreements*: In order to facilitate the development of improvements that are oversized in relationship to a specific development, the County may enter into a reimbursement agreement with the developer to return non-project specific costs over a



period of time. Reimbursement agreements may be utilized for projects such as sewer lines, road improvements, or any other infrastructure improvements not directly related to a specific project, but essential to the improvement of the whole community.

- c. *Development Agreement*: Development Agreements allow for a developer/land owner to obtain vested rights for a proposed development in exchange for an agreement to make infrastructure investments. Developer Agreements are attractive due to their flexibility. The County may ask for infrastructure investment beyond limitations established through the Subdivision Process or through other County regulations.

2. Countywide Impact Fees

Impact fees are assessed to all new residential, commercial, and industrial development. As of 2007, impact fees have been adopted for countywide public services, including general government, public protection, libraries, parks, Sheriff patrol and investigation, and fire services. Impact fees for roads have also recently been adopted. Road impact fees are in place to cover the cost of road improvements necessary to serve new development. School impact fees are also assessed to new development and are administered by the Chowchilla Elementary School District and the Chowchilla High School District.

3. Federal & State Grants

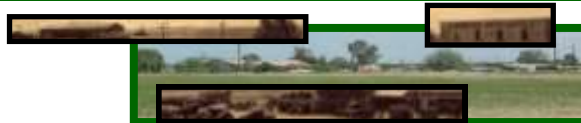
The adoption of any long-range plan, including an Area Plan, is attractive to grant agencies. Once a Plan is in place that identifies improvements needed and how those improvements should be implemented, state and federal governments are more confident that their grants will be most useful. Furthermore, the need for grant monies in Fairmead has been clear for decades due to the poor condition and lack of infrastructure and services. Grants can be pursued to address many issues, including various infrastructure improvements and community facilities and services. Active grant agencies include the U.S. Department of Agriculture (for rural development), Environmental Protection Agency, and the State Department of Housing and Community Development.

4. Measure T

Adopted as a County proposition in 2006, measure T creates a ½ cent sales tax for the maintenance of existing road facilities and capital improvements. Similar to impact fees, revenues are distributed countywide. However, the creation of an Area Plan will identify specific projects that may qualify for limited Measure T funding.

Continuing Community Involvement

The community of Fairmead has actively petitioned for community improvements from the County since community meetings for the Area Plan were initiated in 2007. Since that time, the community group, Fairmead Community and Friends, formed to help give a voice to the community and serve as a body organized to achieve targeted community improvements. To ensure the community's role in the implementation of the Area Plan, a recognized body should be formed to review all discretionary projects in the planning area, as well as any grant activity sought for the



community. Such a recognized body is vital to strengthening the community’s role and sense of ownership regarding future development.

Financing & Implementation – Goal and Policies

Goal: Ensure efficient coordination, targeted financing efforts, and active community involvement within the planning area.

Objective FI-1: Provide several options to allow for funding and coordination in furthering community improvements.

Policy 1: The County shall work with developers to incorporate off-site improvements that are beneficial to the entire community. Useful tools include development agreements or reimbursement agreements.

Policy 2: Public-private partnerships are strongly encouraged as a viable option to leverage funding for various community-wide improvements.

Policy 3: The County shall actively pursue federal and state grant money to address infrastructure, service, and housing needs. Grant activity shall target issues and programs as identified in this Plan.

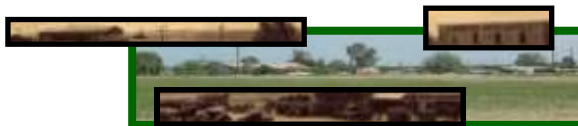
Policy 4: Needs and programs as identified in this Plan shall be given high priority for capital improvements funded with countywide impact fee revenue.

Objective FI-2: Ensure improved communication and coordination between Madera County and local school districts as early as possible in the development process.

Policy 5: The County shall strive to allow for school facilities, including but not limited to permanent and interim school facilities, district-wide support facilities, and transportation and bussing facilities to be available concurrent with the need for such school facilities. The County will work with the Chowchilla Elementary School District and the Chowchilla Union High School District to the extent legally feasible, including adherence to Government Code section 65352.2.

Policy 6: Enhanced measures as provided through policy 34, Chapter 6, Community Facilities, Services, and Infrastructure and policies 7 through 9 of Chapter 8, Financing and Implementation, are contingent upon the school districts providing documentation to the Planning Commission and Board of Supervisors that adequate school facilities cannot be made available concurrent with the need for such facilities. The documentation shall include, but is not necessarily limited to, all of the following:

- a) The school district has imposed school mitigation fees pursuant to *Government Code* Section 53080 and said fees are not adequate to address school facility impacts;



- b) The school district has filed a copy of its adopted School Facilities Master Plan with the Planning Department; and
- c) The school district demonstrates that it has pursued and exhausted all other legally permissible means of providing adequate facilities to serve the development project.

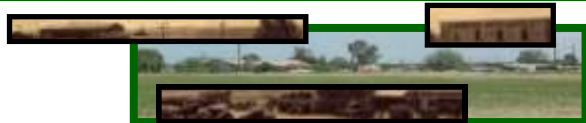
Policy 7: Prior to the filing of any subdivision map, conditional use permit, site plan review, rezone, or general plan amendment, the Planning Department will send all preliminary application documents to potentially affected County Departments, the Chowchilla Elementary School District, and the Chowchilla Union High School District for review and comment. All agencies/departments that wish to provide formal comment to the Planning Department must do so within 30 days of receipt of the preliminary application documents.

Policy 8: Upon receiving any pre-application comments, the applicant may request a meeting with any combination of County Departments, the Chowchilla Elementary School District, or the Chowchilla Union High School District. The Chowchilla Elementary School District or the Chowchilla Union High School District may also request a meeting with the project applicant. Within 15 days of receiving such request from either the applicant or either school district, the Planning Department shall hold the desired meeting at the Planning Department.

Policy 9: The Chowchilla Elementary School District and/or the Chowchilla High School District may ask for the following information and/or comment on the following during the pre-application process:

- a) New residential subdivisions that are large enough to include school sites may be requested to dedicate land and/or pay fees in lieu of dedication for the acquisition and development of school site(s) per the adopted school facilities master plan of the applicable school district(s). The school districts may furnish a report that shows if the districts can accommodate students generated by the proposed development project in schools located within the Fairmead Area Plan area or other options that are legally permissible have been exhausted.
- b) The school districts may furnish a report regarding whether or not the school districts a) have adequate school facilities to serve the project; b) have in place the financing of such facilities that may be required for the project; -c) have determined a phasing that accommodates the school facility needs of the project in accordance with the applicant; or d) have determined other measures legally permitted may be implemented by the County or the applicable school district(s).

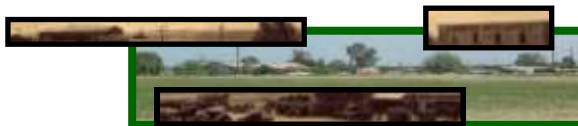
Policy 10: Following receipt of a complete application for any subdivision map, conditional use permit, site plan review, rezone, or general plan amendment, the Chowchilla Elementary School District and/or Chowchilla Union High School District may request a meeting with the County Planning Department to discuss possible methods of coordinating planning, design, and construction of new school facilities and school sites in coordination with the existing or planned infrastructure, land use, and zoning designations of the County. If a meeting is requested, the County Planning Department shall meet with the school district within 15 days following notification. At any meeting requested, the County and Districts may review and



consider, but are not limited to, the following issues as provided by Government Code Section 65352.2:

- a) Methods of coordinating planning, design, and construction of new school facilities and school sites in coordination with the existing and planned infrastructure, land use, and zoning designations.
- b) Options for the siting of new schools
- c) Methods of maximizing the safety of persons traveling to and from school sites.
- d) Opportunities to coordinate the potential siting of new schools in coordination with existing or proposed community revitalization efforts by the County.
- e) Opportunities for financial assistance which the County may make available to assist the school district with site acquisition, planning, or preparation costs.
- f) Review all possible methods of coordinating planning, design, and construction of new school facilities and school sites or major additions to existing school facilities and recreation and park facilities and programs in the community.

Policy 11: Subsequent to any meeting held between the County and the school districts pursuant to Government Code Section 65352.2, the parties involved are encouraged to provide a summary of issues discussed and points of agreement and/or disagreement. The Planning Department shall keep all such documentation of meetings and include such documentation in all project related reports to the Planning Commission and/or Board of Supervisors.



Objective FI-3: Allow for efficient administration of the Area Plan in the long term.

Policy 12: If portions of the Area Plan are deemed no longer enforceable, the remaining provisions, or the remaining application of those provisions will continue as enforceable policy. The Planning Director is authorized to enforce remaining Area Plan language as necessary. The Area Plan is consistent with the Madera County General Plan and it cannot be amended or required to be amended as a result of the implementation of any policies contained with this Area Plan.

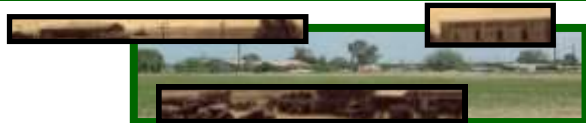
Policy 13: The Zoning Administrator has the authority to interpret the final designation of all land use boundaries as depicted within the land use plan and consistent zoning district boundaries. This policy does not pertain to the location of school facilities.

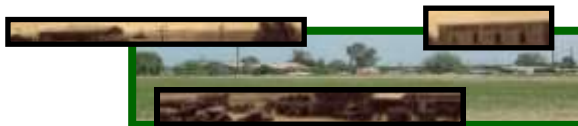
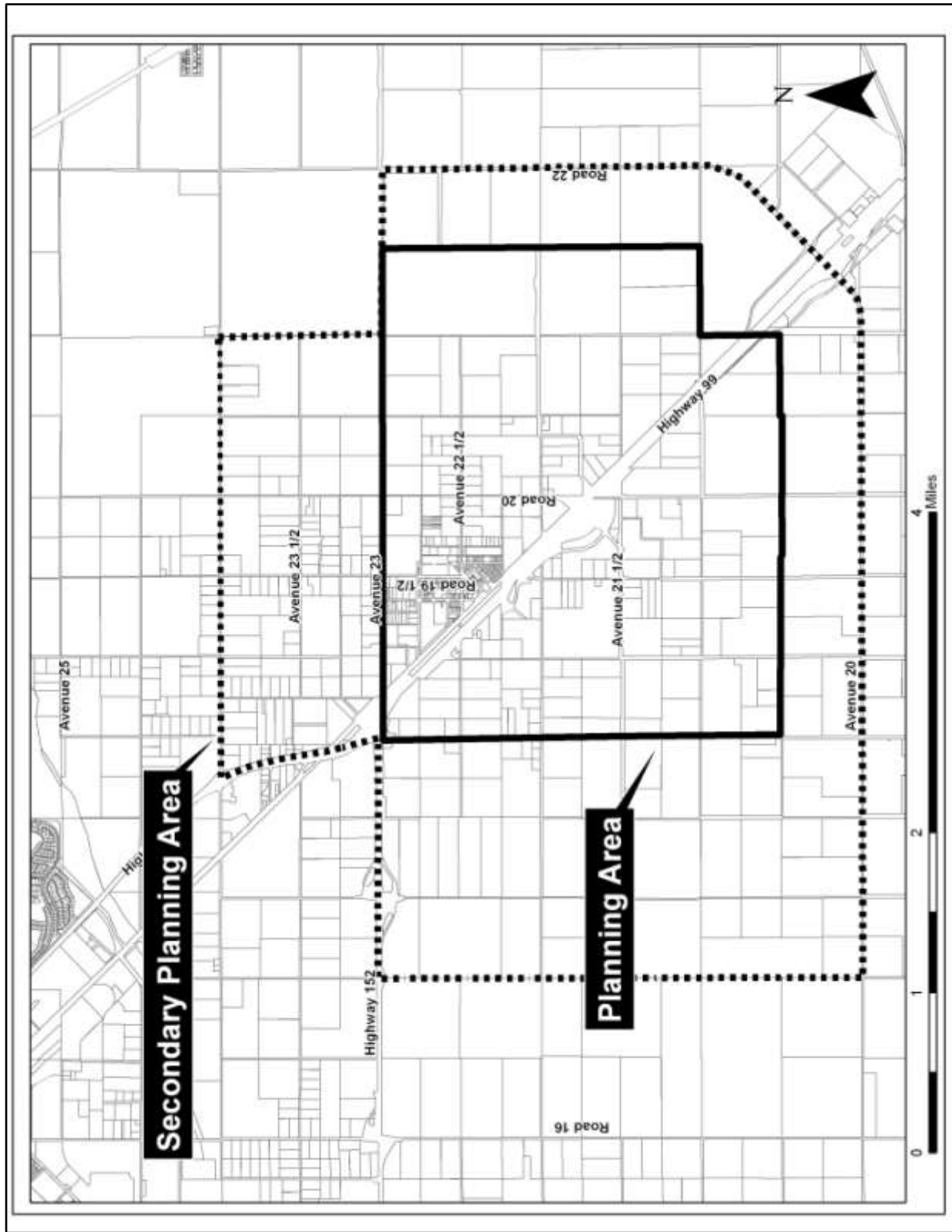
Objective FI-4: Provide avenues for the active involvement of the community in planning area and surrounding development decisions.

Policy 14: Strengthen the community's role and sense of ownership in implementing the area plan by establishing an active Community Advisory Council or Area Plan Implementation Committee. Allow community group review and comment on all discretionary projects.

Policy 15: Encourage developers to meet with the community prior to application submittal to the County.

Policy 16: Require all discretionary projects proposed within the secondary planning area to obtain comments from both the County of Madera and the Community Advisory Council or Area Plan Implementation Committee (as authorized through Policy 1). The secondary planning area is shown on page 69.





CHAPTER ~ 9 ~

General Plan Consistency

The Fairmead Colony Area Plan implements several policies from the Madera County General Plan. In fact, the General Plan gives specific direction for the creation of a Plan for the Fairmead area. Land Use implementation program 1.1 highlights the need to adopt plans for a number of areas within Madera County for the provision of local planning and community facility and service issues. Fairmead is noted as an area of need for such a plan.

Residential Land Use

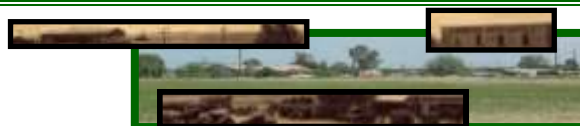
General Plan Goal: To provide adequate land in a range of residential densities to accommodate the housing needs of all income groups expected to reside in Madera County.

Area Plan Provision: The Land Use Plan can accommodate over 2,400 residential units of various densities (including existing units). A change in the area's zoning regulations will match the new land use designations, allowing for consistency. Higher densities will allow for more affordability. The area of planned development includes medium and high density development (high density included within mixed use areas) lies at the junction of Highway 152 and Highway 99 and will be easily accessible to both routes. Multi-family housing, including medium and higher density residential areas is concentrated in and around the village core as designated in the Land Use Plan. The Fairmead Colony Area Plan utilizes Highway 99 and the Union Pacific Railroad as a buffer to separate light industrial uses, the Fairmead Landfill, and the long-term sewer treatment plant from the core community area. On the eastern edge of the community area, a vegetative buffer is incorporated to minimize pesticide drift from adjacent agricultural areas. The circulation system as designed maximizes access for all modes of transportation within and without residential areas. Residential blocks are sized small to curtail the use for cul-de-sacs and maximize accessibility for all modes of transportation. Independent multi-use paths increase bicycle and pedestrian connectivity within residential areas.

Commercial Land Use

General Plan Goal: To designate adequate commercial land for and promote development of commercial uses to meet the present and future needs of Madera County residents and visitors and maintain economic vitality.

Area Plan Provision: The commercial core, also known as the village core, is located adjacent to the junction of Highways 99 and 152. Such a location is ideal for visibility and should enhance the overall attractiveness of the village core. A community center, fire station, and public parks are planned for



the village core. Mixed use land use will allow for cafes and sidewalk shops to accommodate pedestrian traffic. The village core is designed to accommodate pedestrian circulation, due to its location at the heart of the planned community and adjacent public uses that will also attract pedestrian traffic. Multi-use paths and multiple streets connect the village core to residential areas that directly abut the village core. The Land Use Plan designates Professional Office adjacent to the new Avenue 21^{1/2}/Road 20 interchange, buffering a highway commercial area from residential area. The Professional Office area lies directly adjacent to a neighborhood center that provides limited commercial activity and denser residential development within easy walking distance.

Industrial Land Use and Economic Development

General Plan Goal: To designate adequate land for and promote development of industrial uses to meet the present and future needs of Madera County residents for jobs and maintain economic vitality.

Area Plan Provision: The Plan promotes industrial development that is located adjacent to Highway 99, with Highway 99 as a suitable buffer from nearby residential uses. The Plan calls for the provision of infrastructure, community facilities, and services that can support employment generators in the future. Land uses are designated to allow for primary wage-earner jobs, including the Professional Office designation.

Jobs-Housing Balance

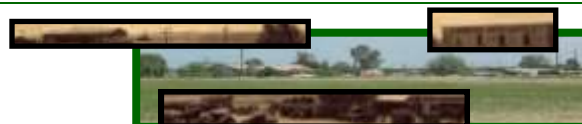
General Plan Goal: To work toward a jobs-housing balance in existing urban areas and new growth areas.

Area Plan Provision: Revitalization of the township of Fairmead is a priority of the Plan, including growth within and around the existing township. Growth shall be intensified, including higher density, compact development centered around a village core. Services are planned to be expanded greatly in order to facilitate the growth of a healthy community. Employment generating uses, including Professional Office, Neighborhood Commercial, and Mixed Use Commercial areas are all located within walking distance of most of the planned and existing residential development.

Roadway Design, Network, and Financing

General Plan Goal: To provide for the long-range planning and development of the County's roadway system, ensure the safe and efficient movement of people and goods, and provide sufficient access to existing and new development.

Area Plan Provision: Per County ordinance, the entire road system will have the proper traffic safety measures implemented. Furthermore, additional improvements including roundabouts, traffic circles, and other traffic calming devices will be utilized to increase not only traffic safety, but multimodal safety. The old residential streets with wide landscaped medians designed into the existing community shall be carried through into new developed areas, thus continuing existing design and improving on existing landscaping. Landscaped areas of road right of way shall be maintained by an all-inclusive County Service District that includes landscaping maintenance.



A high level of service will be maintained with the following provisions: 1) an efficient grid street system that features smaller than normal blocks to increase pedestrian access and decrease the length of dead-end roads. 2) A multi-use path system that increases bicycle and pedestrian accessibility as a system independent of the street system. 3) Land uses that are designed to fit together in a close-knit, compact system that increases pedestrian and bicycle opportunities.

Several major highway improvements are a part of the Plan, including the extension of Highway 152 east from Highway 99 to act as the northern boundary of the planning area and the recent development of a new interchange at Road 20/Avenue 21 ½ along Highway 99, providing direct access into the planning area. A system of arterial and connector roadways are designed into the Circulation Plan to accommodate through traffic and discourage the heavy use of local and residential streets.

The Plan calls for the funding of circulation improvements via a diverse range of possible federal, state, and local funding resources. The Plan identifies necessary improvements ahead of new development. All improvements required of new development will be based off the Subdivision Map Ordinance in conjunction with the Area Plan.

Transit--General

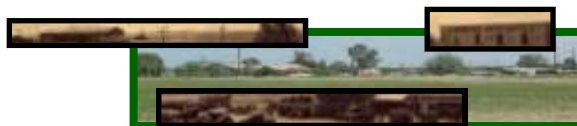
General Plan Goal: To promote a safe and efficient mass transit system, including both rail and bus, to reduce congestion, improve the environment, and provide viable non-automotive means of transportation in and through Madera County.

Area Plan Provision: The County will work with the Madera Transportation Commission to coordinate the long-term provision of bus service to and from the Fairmead Area. Both the village core and Gateway District have identified areas suitable for bus stops. Bus stops will consist of pull-out areas with public seating and shade. The Circulation Plan identifies appropriate sites for park and ride facilities adjacent to Highway 99 and 152.

Transportation Control Measures

General Plan Goal: To maximize the efficient use of transportation facilities so as to: 1) reduce travel demand on the County's roadway system; 2) reduce the amount of investment required in new or expanded facilities; 3) reduce the quantity of emissions of pollutants from automobiles; and 4) increase the energy-efficiency of the transportation system.

Area Plan Provision: The Plan directly encourages transportation control measures, including the use of park and ride lots and a reduced street network scale that promotes pedestrian and bicycle travel. Land uses are concentrated into a compact, mostly walkable town that will be self-dependent from the rest of the County. The land use and circulation system design encourages other forms of travel apart from the automobile.



Non-motorized Transportation

General Plan Goal: To provide a safe, comprehensive, and integrated system of facilities for non-motorized transportation to meet the needs of commuters and recreational users.

Area Plan Provision: The Plan identifies multi-use paths that allow both pedestrian and bicycle use throughout the entire planning area. This system is independent of the street network. All streets will also be constructed to handle bicycles and pedestrians. Developers who wish to develop adjacent to or around the identified green path area will be required to construct the associated section. The multi-use path system (labeled “green paths”) will be connected to parks. Parks shall be located adjacent to green paths to maximize recreational opportunities and connections.

Community facilities and Services

General Plan Goal: To ensure the timely development of community facilities and to maintain an adequate level of service to meet the needs of existing and future development.

Area Plan Provision: The Plan identifies and plans for sewer, water, and drainage system designs based on planning area build out. All of these systems must be improved as new development occurs within the planning area.

Water Supplies

General Plan Goal: To ensure the availability of an adequate and safe water supply and the maintenance of high quality water in water bodies and aquifers used as sources of domestic and agricultural water supply.

Area Plan Provision: New sewer treatment facilities must be equipped with tertiary treatment. Through tertiary treatment, the use of reclaimed water is required for landscaping of public right of way and new development. Reclaimed water will be made available for agricultural re-use.

Wastewater

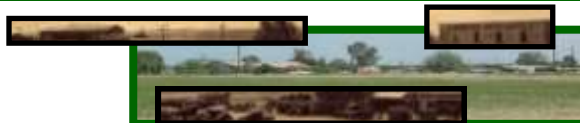
General Plan Goal: To ensure adequate wastewater collection and treatment and the safe disposal of liquid and solid waste.

Area Plan Provision: Water-conserving design is encouraged in all new developments in the planning area.

Storm Drainage and Flood Control

General Plan Goal: To provide efficient, cost-effective, and environmentally-sound storm drainage and flood control facilities.

Area Plan Provision: A Drainage Master Plan has been developed that accounts for drainage within the entire planning area. Developers must make improvements in relation to the percentage of



planning area developed. Retention basins and both Fairmead Creeks handle drainage and provide recreational opportunities for residents.

Fire Protection Services

General Plan Goal: To protect residents of and visitors to Madera County from injury and loss of life and to protect property and watershed resources from fires.

Area Plan Provision: New development shall contribute towards the development of a fire station within the village core of the planning area. The fire station will also include an ambulance.

Schools

General Plan Goal: To provide for the educational needs of Madera County residents.

Area Plan Provision: The Area Plan provides flexibility to allow for the priorities of the Chowchilla Elementary and Chowchilla Union High School Districts to be met. "Floating school" sites have been identified within the Area Plan to allow the school districts a surety that areas may be preserved to allow for school facilities as development occurs. Policies are included within the Area Plan to allow school facilities to be available concurrent with the need for school facilities. For example, policies are incorporated to ensure coordination is achieved with the school districts in accordance with Government Code section 65352.2. Chapter 8, Financing and Implementation, includes many policies necessary for school districts to provide services concurrent with new development. Language specifically attentive to school facility siting and circulation pertaining to schools has been included to allow for schools to be safely and efficiently designed into the surrounding community.

Public Recreation and Parks

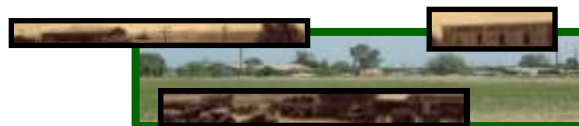
General Plan Goal: To designate land for and promote the development and expansion of public and private recreational facilities to serve the needs of residents and visitors.

Area Plan Provision: New development within the planning area must provide park space or fees in lieu of park space, per the Quimby Act. Park space should be developed as identified in the Land Use Plan, in addition to common areas within individual developments. Park space maintenance will be funded in part through a Community Service District that encompasses the entire planning area. New school development should consider joint-use opportunities with green path easements and park space.

Historical and Cultural Resources

General Plan Goal: To identify, protect, and enhance Madera County's important historical, archaeological, paleontological, and cultural sites and their contributing environment.

Area Plan Provision: The Area Plan supports the relocation of the Mammoth Orange Hamburger Stand to the Highway 99/Road 20 interchange to make the township and the Orange more attractive to visitors. Land uses in the area will be compatible with the Orange Hamburger Stand.



Agriculture

General Plan Goal: To designate adequate agricultural land and promote development of agricultural uses to support the continued viability of Madera County's agricultural economy.

Area Plan Provision: This Plan proposes to build within and around an existing and formerly developed area. Growth is planned to be compact and discourages leap frog development into prime farmland. Farmland is planned to be preserved on the outer fringes of the planning area to maintain the agricultural character of the area and to preserve prime farmland. The Plan encourages agriculturally designated areas not planned for growth to remain within the Agricultural Preserve Program. A green path with heavy landscaping will act to buffer new residential areas from adjacent agricultural properties.

Air Quality

General Plan Goal: To protect and improve air quality in Madera County and the region.

Area Plan Provision: This Plan reflects new urbanist style development--development that is compact with mixed uses to reduce automobile transportation and encourage walkability. A community is promoted that provides all of the services residents need, thus relieving the necessity to drive to Madera or Chowchilla for goods and services.

Air Quality--Transportation/Circulation

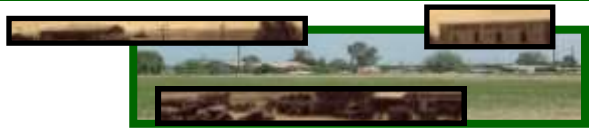
General Plan Goal: To integrate air quality planning with the transportation planning process.

Area Plan Provision: A compact grid network increases access for all uses, increasing intra- and inter-neighborhood connections and parallel roadways. The green path system and the compact grid system both encourage walking and bicycling.

Flood Hazards

General Plan Goal: To minimize the risk of loss of life, injury, damage to property, and economic and social dislocations resulting from flood hazards.

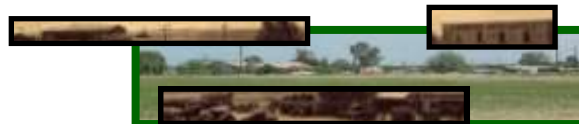
Area Plan Provision: A Master Drainage Plan will be developed in conjunction with the Plan. The Drainage Plan is designed to mitigate current flood zone problems on the east side of the planning area, utilizing a stream system and retention basins that incorporate recreational opportunities.



Public Safety and Emergency Management Facilities

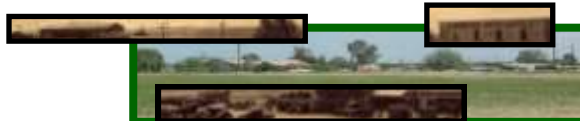
General Plan Goal: To protect public health and safety through safe location of structures necessary for the protection of public safety and/or the provision of emergency services.

Area Plan Provision: The planned site for the fire/ambulance station is not within any flood zone or risk area.



Other Plans

Apart from the County General Plan, no other local plans are effective with the planning area. The State Department of Transportation's Route 99 Corridor Enhancement Master Plan, however, is currently being implemented with existing Route 99 right of way. Recent implementation includes the widening of Highway 99 and the installation of the Road 20/Avenue 21 ½ interchange. Moreover, the Plan is intended to promote an improved unity between landscaping and aesthetics.



DEFINITIONS

Community facilities and services: includes churches, meeting halls, public services (fire stations, post offices, police stations, etc.), educational institutions (permanent and interim schools, school district-wide support facilities, and other related facilities) museums, libraries, government facilities, hospitals, and other uses that serve the public.

Conditional Use Permit (CUP): allows the County to consider special uses which may be essential or desirable to a particular community, but which are not allowed as a matter of right within a zoning district, through a public hearing process. Consideration of a CUP is a discretionary act.

Cottage-style housing: smaller single family detached homes built in clusters, close together, that have some common area.

Floating school site: the school symbols as shown on the land use map indicate the general location for future schools to be located within the Area Plan. The symbols are not intended to indicate an exact location or property. The local school districts have authority to locate schools on any land use designation within the Area Plan and the floating school site designation allows for this flexibility as acknowledged by the County.

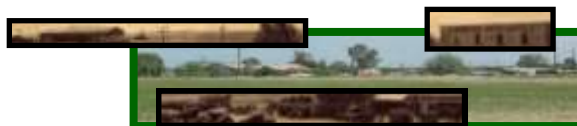
Garden apartments: a low-rise apartment complex that includes a substantial amount of open area, usually landscaped ground, which is utilized as a community common area.

Greywater: is wastewater generated from domestic activities such as laundry, dishwashing, and bathing, which can be recycled on-site for uses such as landscape irrigation. Greywater differs from water from toilets which is designated sewage or blackwater to indicate it contains human waste.

Infrastructure: includes sewer and water systems, street lighting, sidewalks, roads, and drainage facilities.

Interim school facilities: Interim school facilities include facilities that are not permanent and are required by the District in the interim of the district financing and developing new permanent facilities. They can include, but are not limited to classrooms, restroom facilities, food service, offices or other types of facilities. In light of the gap between the cost of permanent school facilities and available financing, there may be the need to provide such interim facilities. As an example, the district might pursue a relocatable school in lieu of permanent facilities simply based on the available of adequate financing. Interim facilities can also include district-wide support facilities, and bussing and transportation facilities.

Local Agency Formation Commission (LAFCO): an independent government entity with the responsibility to decide boundary issues pertaining to city and county (non-incorporated) lands, including spheres of influence, and issues about the annexation of county lands into a city or special district.



Madera Action Partnership: also referred to as the Community Action Partnership of Madera County, is a non profit agency that serves more than 5,000 low and moderate income residents annually through a broad spectrum of programs and resources.

Madera County Transportation Commission (MCTC): the Commission serves as the Regional Transportation Planning Agency (RTPA) and the designated Metropolitan Planning Organization (MPO) for Madera County. The Commission is responsible for the development and adoption of the Regional Transportation Plan and the Transportation Improvement Program as required by state law. The Commission serves as a clearinghouse for Countywide transportation funds.

Planning area: the area subject to an Area Plan or Specific Plan.

Project or development: any discretionary application filed with the County that will result in the development of property within the Area Plan including but not limited to all or part of tentative and final subdivision maps, general plan amendment, area plan amendment, rezoning, or any other land use development authorization subject to discretionary approval.

Public and quasi-public uses: includes churches, meeting halls, public services (fire stations, post offices, police stations, etc.), educational institutions (permanent and interim schools, school district-wide support facilities, and other related facilities), museums, libraries, government facilities, hospitals, and other uses that serve the public.

Reclaimed water: highly treated wastewater that can be used for irrigation and other uses.

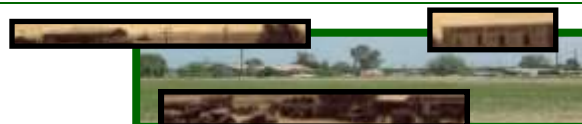
Regional housing assistance agencies: local and state agencies including the Madera Housing Authority, Community Action Partnership of Madera County, and the California Department of Housing and Community Development.

Retention basin: a basin that stores stormwater, wherein the storage of the stormwater is on a more permanent basis. The water often remains in a retention basin indefinitely, with the exception of the volume lost to evaporation and the volume absorbed into the soils.

Roundabout: a type of circular junction in which road traffic must travel in one direction around a central island. Signs and striping usually direct traffic entering the circle to slow down and give the right of way to drivers already in the circle.

Sensitive Receptor: includes, but is not limited to residential units and land uses, educational facilities and schools, medical facilities, places of worship or other land uses that may require a higher level of protection and mitigation from the impacts of construction. These sensitive receptors may be sensitive to noise, vibration, air pollutants, and other conditions that impact our environment. Sensitive receptors located in the vicinities of proposed improvement projects could be impacted by construction and implementation of the proposed highway, arterial and transit projects due to noise, dust, vibration, etc. This could be considered a potentially significant impact.

Sphere of influence: a plan for the probable physical boundaries and service area of a local agency (including the cities of Madera County), as determined by the Local Agency Formation Commission. Includes an adopted sphere of influence boundary.



Swale: a low stretch of land where storm runoff flows. The term can refer to a natural landscape feature or a human-created one. Artificial swales are often designed to manage water runoff, filter pollutants, and increase rainwater infiltration.

Traffic circle: a type of circular intersection in which traffic must travel in one direction around a central island. Traffic circles should not be confused with roundabouts, in which entering traffic must always yield to traffic already in the circle, and generally operate at much lower speeds.

Village Core: a designation in the General Plan within Mixed Use Core (MUC) created to support a community center. Land uses with the Village Core must include 50 percent Medium and High Density Residential, 40 percent Community Commercial, 7 percent Public Institutional, and 3 percent Open Space (for parks and recreation).

Williamson Act: state law that enables local governments to enter into contracts with private landowners for the purpose of restricting specific parcels of land to agricultural or related open space use. In return, landowners receive property tax assessments which are much lower than normal because they are based upon farming and open space uses as opposed to full market value.

